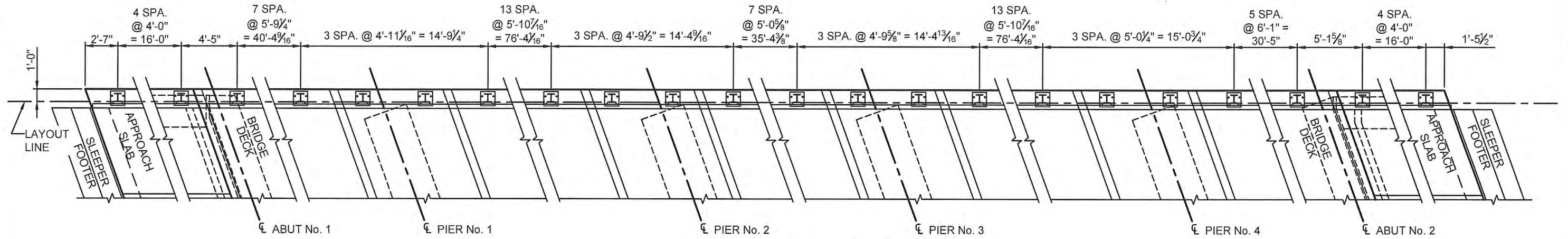




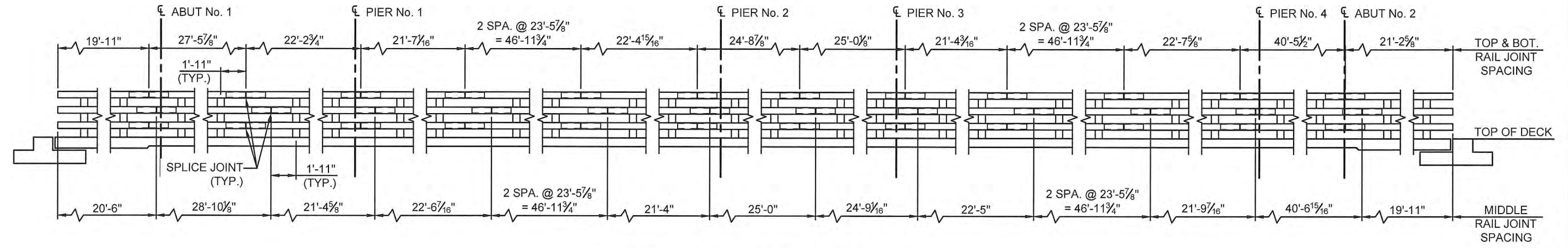
Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

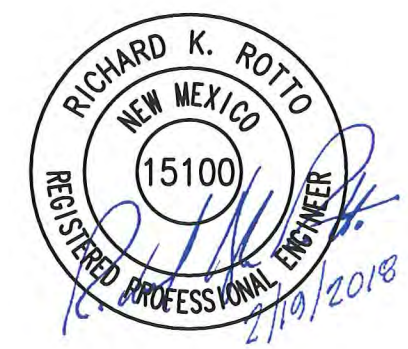
SHEET No.  
5-82



**LEFT BRIDGE RAIL PLAN**  
(SCHEMATIC)



**LEFT BRIDGE RAIL ELEVATION**  
(SCHEMATIC)



REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
3			

NEW MEXICO DEPARTMENT OF TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

NM TYPE A42 METAL  
BRIDGE RAILING  
(LT. SIDE)

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

APPROVED FOR CONSTRUCTION: Kimberly Coleman DATE: 2/20/2018

CALVIN SANCHEZ  
19-Feb-18

Drawing File: G:\NMDOT\2483 - 14-01 BRIDGE ON-CALL\TASK 11 - BR 7345 REHAB\00\_PLANS\BR 7345\_BRIDGE RAILING.DWG  
12:58 PM

NEW MEXICO PROJECT NO. 4101490

STA. 840+48.62

SHEET NO. 5-82

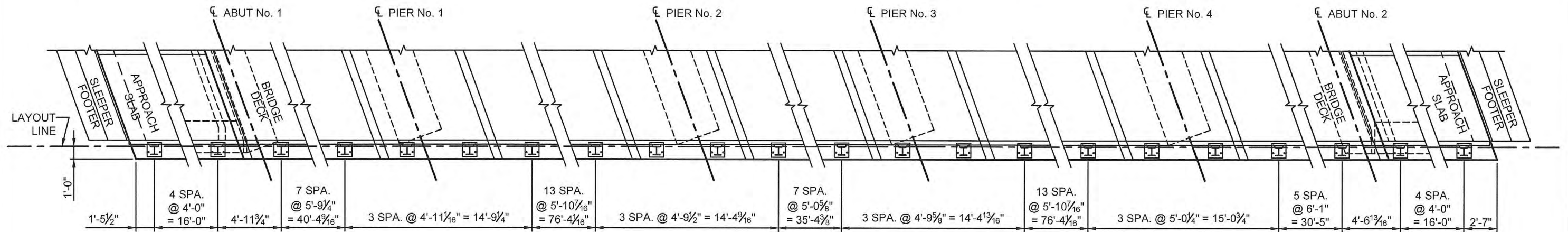
DRAWING SCALE:



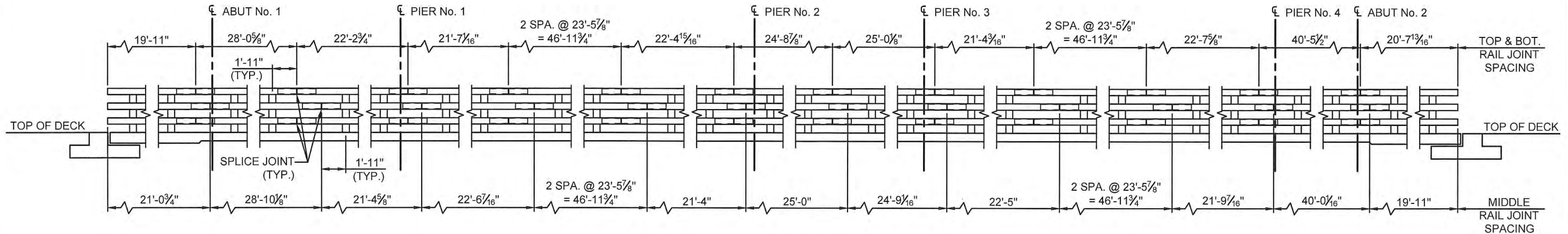
Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

SHEET No.  
5-83



**RIGHT BRIDGE RAIL PLAN**  
(SCHEMATIC)



**RIGHT BRIDGE RAIL ELEVATION**  
(SCHEMATIC)



REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

NM TYPE A42 METAL  
BRIDGE RAILING  
(RT. SIDE)

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

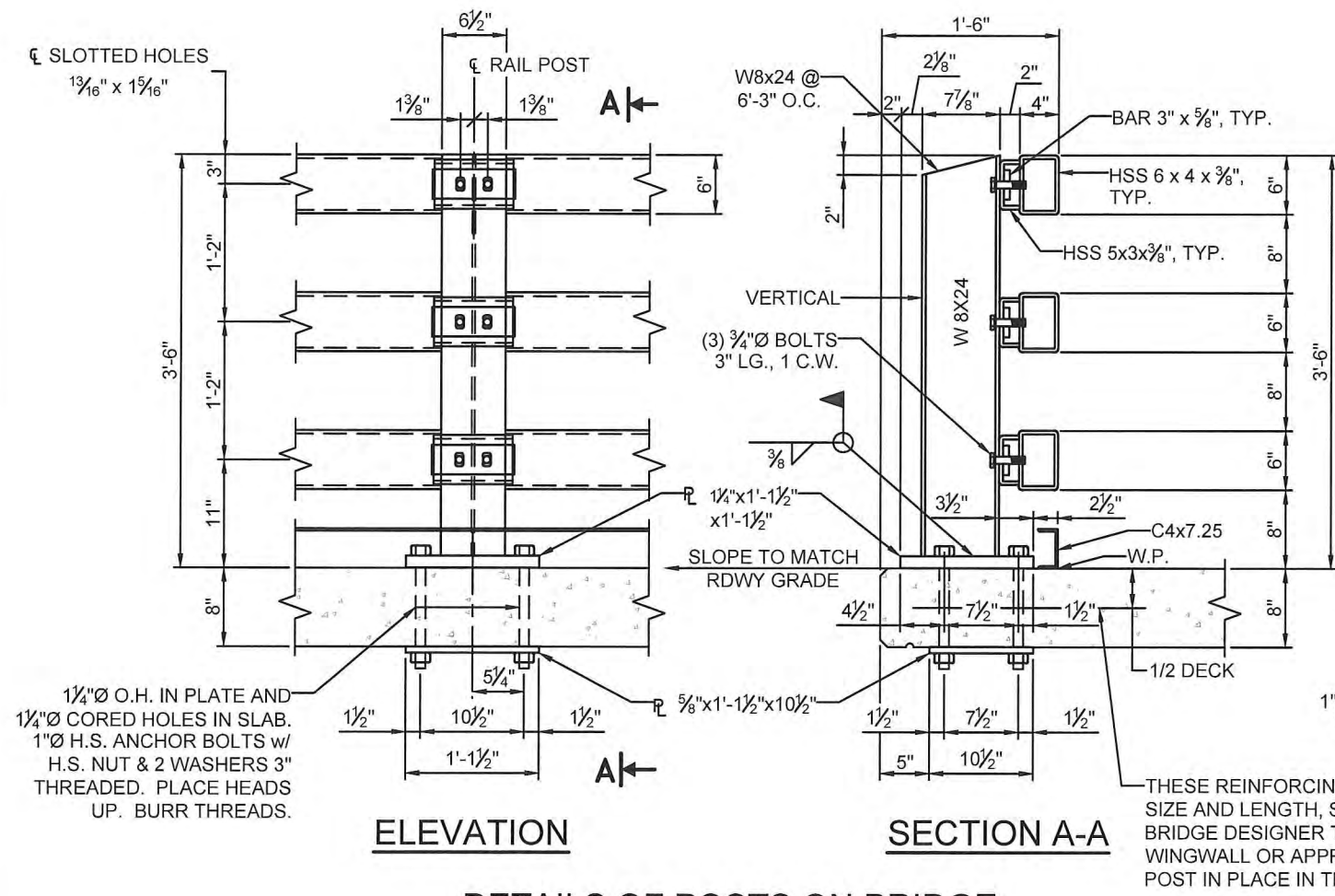
APPROVED FOR CONSTRUCTION: Kimberly Coleman DATE: 2/20/2018



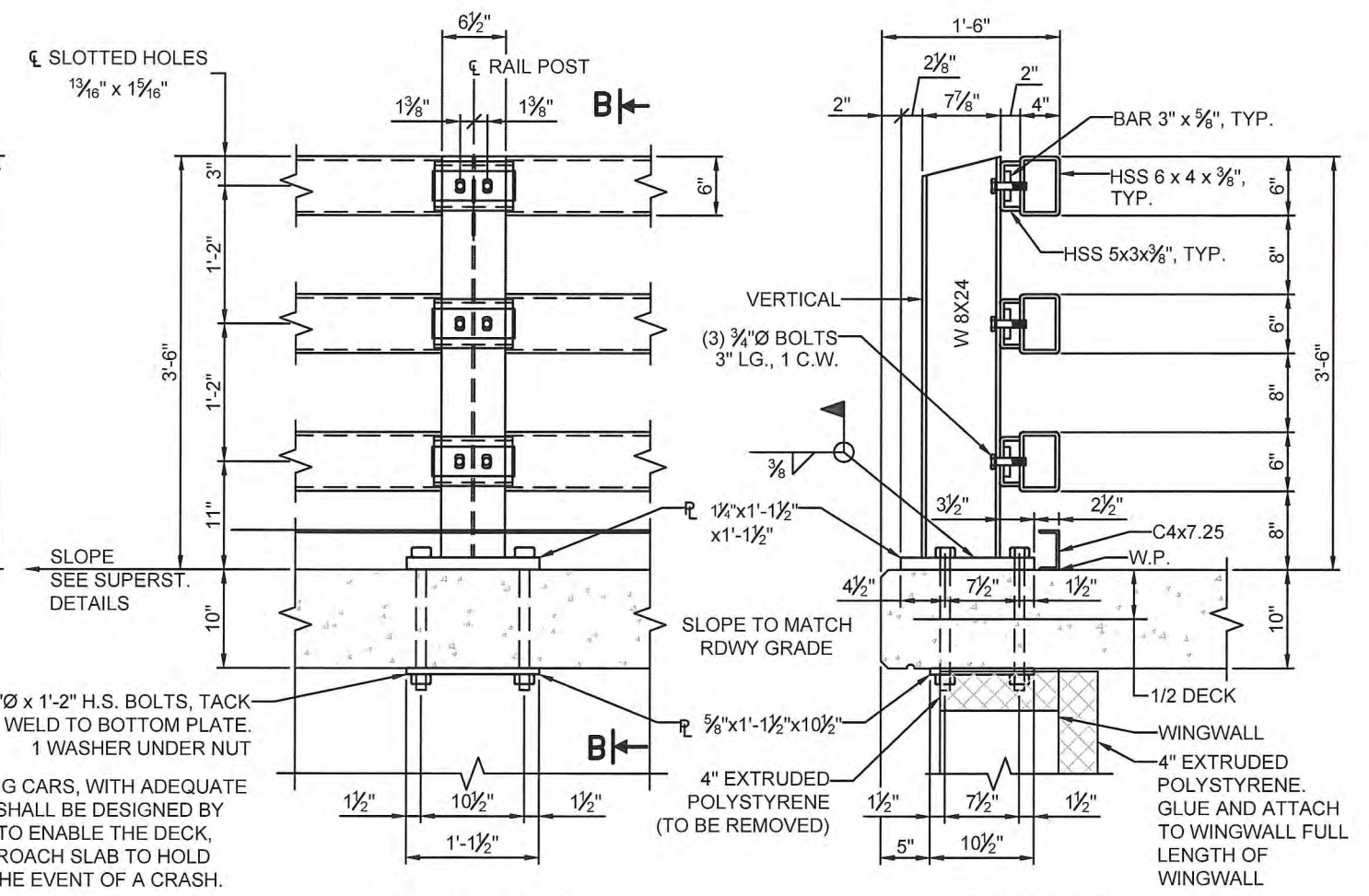
Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

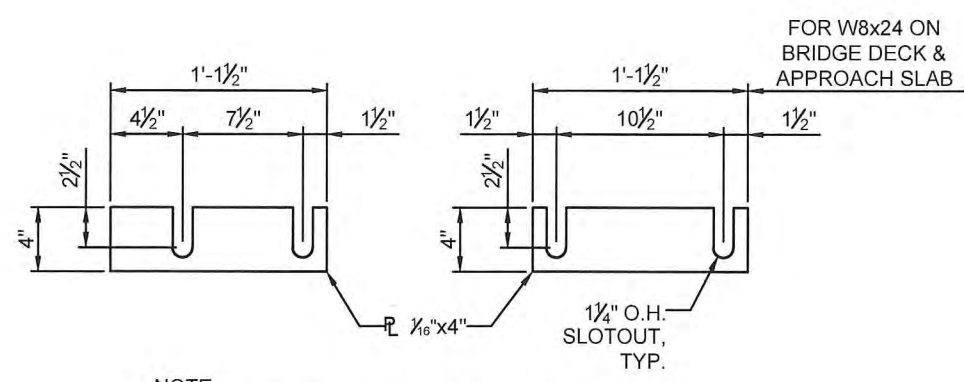
SHEET No.  
5-84



**ELEVATION**  
**SECTION A-A**  
**DETAILS OF POSTS ON BRIDGE**



**ELEVATION**  
**SECTION B-B**  
**DETAILS OF POSTS ON APPROACH SLAB**



NOTE:  
POST SHIMS ARE TO BE USED BETWEEN POST AND  
CONCRETE WHERE NECESSARY TO ALIGN POSTS.  
REQ'D ONE SHIM EACH PER POST

**POST SHIMS**

**NOTES:**

1. THIS SHEET IS PROJECT SPECIFIC AND SUPERSEDES STANDARD DRAWING 543-07-1/4.
2. SEE STANDARD DRAWINGS 543-07-2/4 THRU 543-07-4/4 FOR ADDITIONAL INFORMATION.
3. W8x24 POSTS SHALL BE FIELD WELDED ONTO THE BOLTED BASE PLATES BY AN APPROVED WORK PLAN AND PROCEDURE BY A CERTIFIED WELDER.
4. SUBMIT WORKING DRAWINGS FOR APPROVAL. ALLOW 21 DAYS FOR REVIEW FOR EACH SUBMITTAL.
5. SUBMIT INSTALLATION WORK PLAN, WELDING PROCEDURE, AND WELDER CERTIFICATION FOR REVIEW AND APPROVAL. ALLOW 14 DAYS FOR REVIEW FOR EACH SUBMITTAL.



BRIDGE			
REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
3			

NEW MEXICO DEPARTMENT OF  
TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

METAL RAILING NM TYPE A42  
DETAILS OF POSTS ON BRIDGE  
AND APPROACH SLAB

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

APPROVED FOR CONSTRUCTION: *Kimberly Coleman* DATE: 2/20/2018



Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

SHEET No.  
5-85

### REINFORCEMENT SCHEDULE

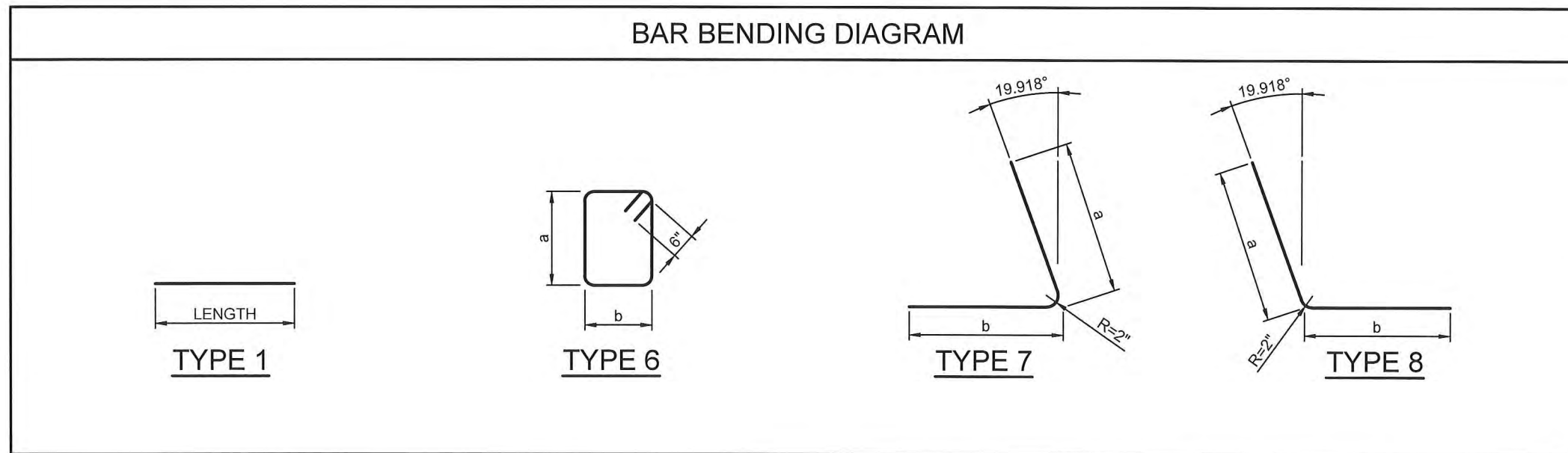
CAST-IN-PLACE					
MARK	TYPE	SIZE	LENGTH	NO. REQ.	REMARKS
<b>SLEEPER FOOTERS</b>					
4AS052	1E	4	5'-2"	128	32 TOP AND BOTTOM PER SLEEPER FOOTER
4AS339	1E	4	33'-9"	24	12 LONGITUDINAL BARS PER SLEEPER FOOTER
4AS066	6E	4	6'-6"	64	32 PER SLEEPER FOOTER, a=1'-6", b= 1'-2"
<b>ABUTMENT 1 BACKWALL</b>					
6AW3511	1E	6	35'-11"	6	
<b>ABUTMENT 2 BACKWALL</b>					
6AW3511	1E	6	35'-11"	4	
<b>ABUTMENT 1 WINGWALLS</b>					
6AW108	1E	6	10'-8"	5	3 BARS FOR RIGHT WINGWALL AND 2 BARS FOR LEFT WINGWALL
6AW132	7E	6	13'-2"	3	FOR RIGHT WINGWALL, a= 10'-3", b= 2'-11"
6AW131	8E	6	13'-1"	2	FOR LEFT WINGWALL, a= 10'-2", b= 2'-11"
6AW0510	7E	6	5'-10"	3	FOR RIGHT WINGWALL, a= 2'-11", b= 2'-11"
6AW0510	8E	6	5'-10"	2	FOR LEFT WINGWALL, a= 2'-11", b= 2'-11"
<b>ABUTMENT 2 WINGWALLS</b>					
6AW108	1E	6	10'-8"	4	2 BARS FOR RIGHT WINGWALL AND 2 BARS FOR LEFT WINGWALL
6AW132	7E	6	13'-2"	2	FOR RIGHT WINGWALL, a= 10'-3", b= 2'-11"
6AW131	8E	6	13'-1"	2	FOR LEFT WINGWALL, a= 10'-2", b= 2'-11"
6AW0510	7E	6	5'-10"	2	FOR RIGHT WINGWALL, a= 2'-11", b= 2'-11"
6AW0510	8E	6	5'-10"	2	FOR LEFT WINGWALL, a= 2'-11", b= 2'-11"

### NOTES:

- DIMENSIONS ARE TO "OUT-TO-OUT" OF BARS.
- BAR TYPES WITH AN "E" DENOTE EPOXY COATED BARS.
- REINFORCEMENT COST SHALL BE PAID FOR IN "ITEM No. 540160 - EPOXY COATED REINFORCING BARS GRADE 60".
- BAR DETAILING SHALL BE IN ACCORDANCE WITH CHAPTER SIX OF THE CONCRETE REINFORCING STEEL INSTITUTE (CRS) MANUAL OF STANDARD PRACTICE, 2009 EDITION.



### BAR BENDING DIAGRAM



REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
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NEW MEXICO DEPARTMENT OF  
TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

REINFORCING BAR  
SCHEDULES  
(CAST-IN-PLACE CONCRETE)

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

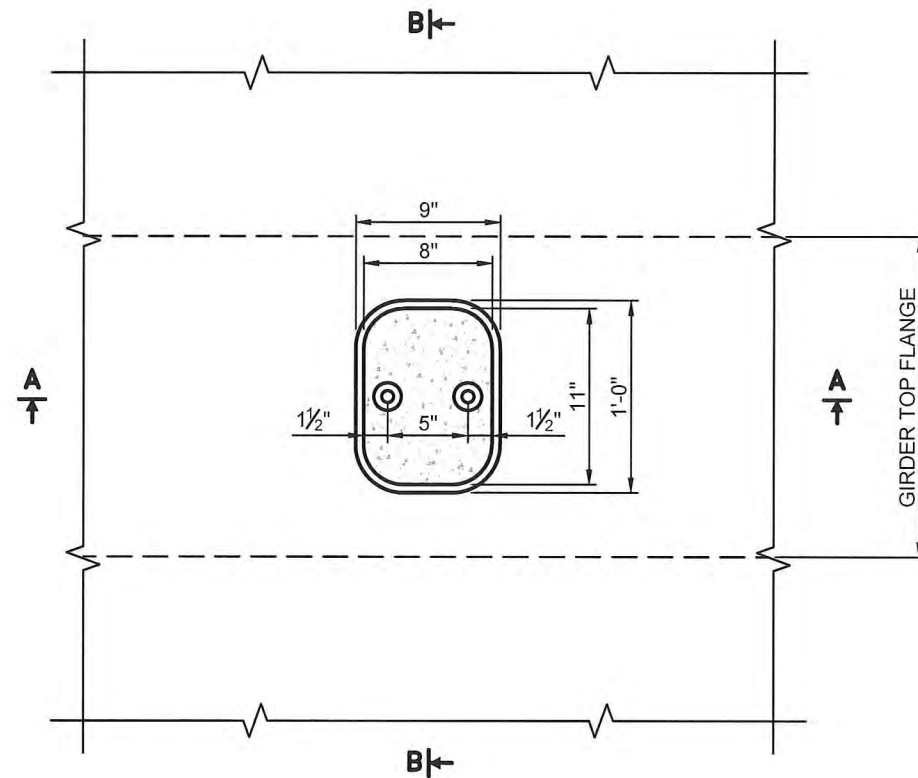
APPROVED FOR CONSTRUCTION: Kimberly Coleman DATE: 2/20/2018



Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

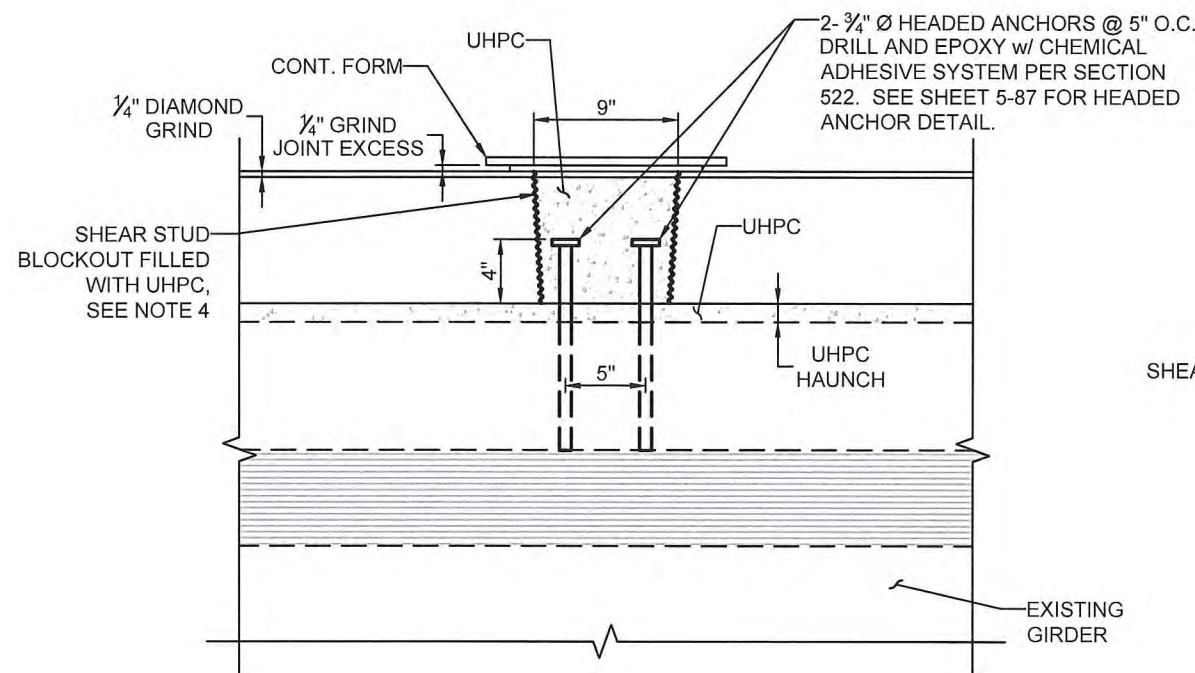
SHEET No.  
5-86



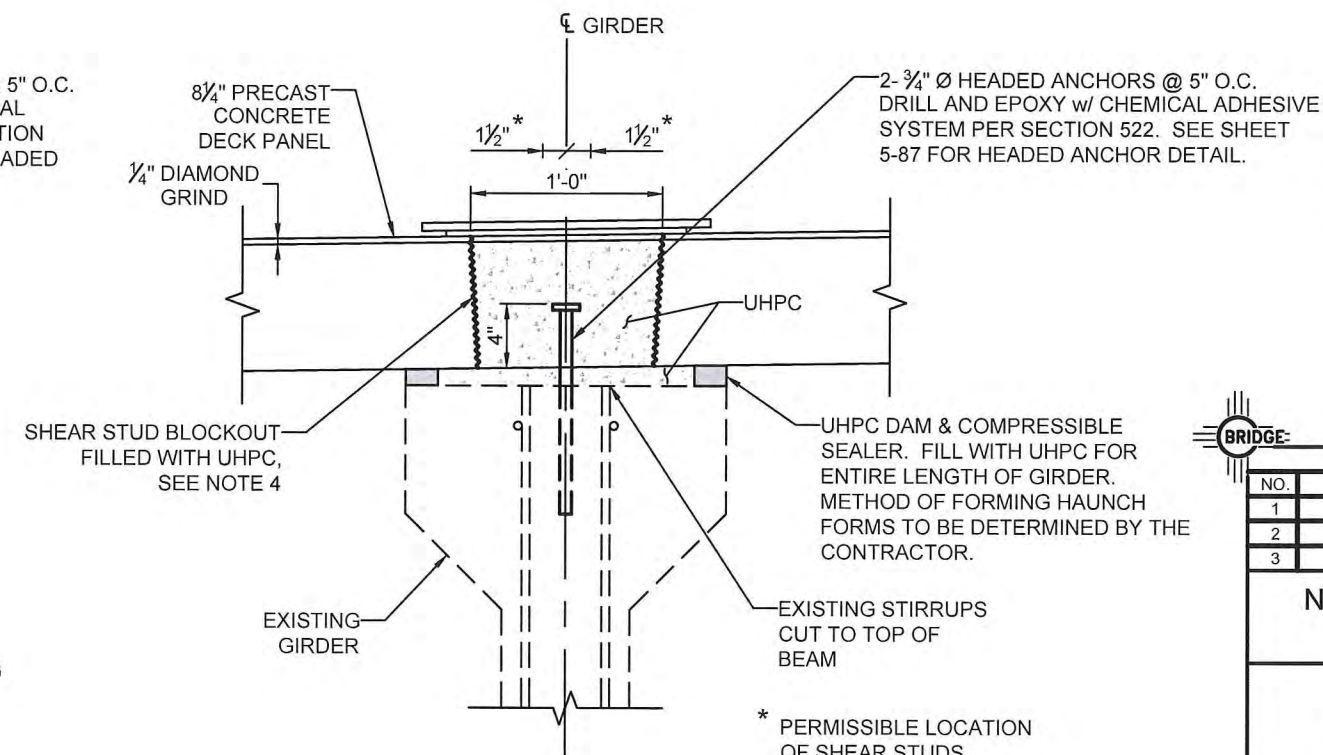
PLAN

**NOTES:**

1. METHOD OF FORMING BLOCKOUT SHALL BE DETERMINED BY THE FABRICATOR.
2. SUBMIT WORKING DRAWINGS FOR HEADED ANCHORS FOR REVIEW AND APPROVAL 30 DAYS PRIOR TO ORDERING MATERIALS.
3. CONCURRENT WITH WORKING DRAWINGS, SUBMIT CHEMICAL ADHESIVE SYSTEM FOR REVIEW AND APPROVAL.
4. BLOCKOUT SHALL HAVE AN EXPOSED AGGREGATE FINISH, ROUGHENED TO 1/4" AMPLITUDE.



SECTION A-A



SECTION B-B



REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
3			

NEW MEXICO DEPARTMENT OF  
TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

UHPC BLOCKOUT  
DETAILS

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

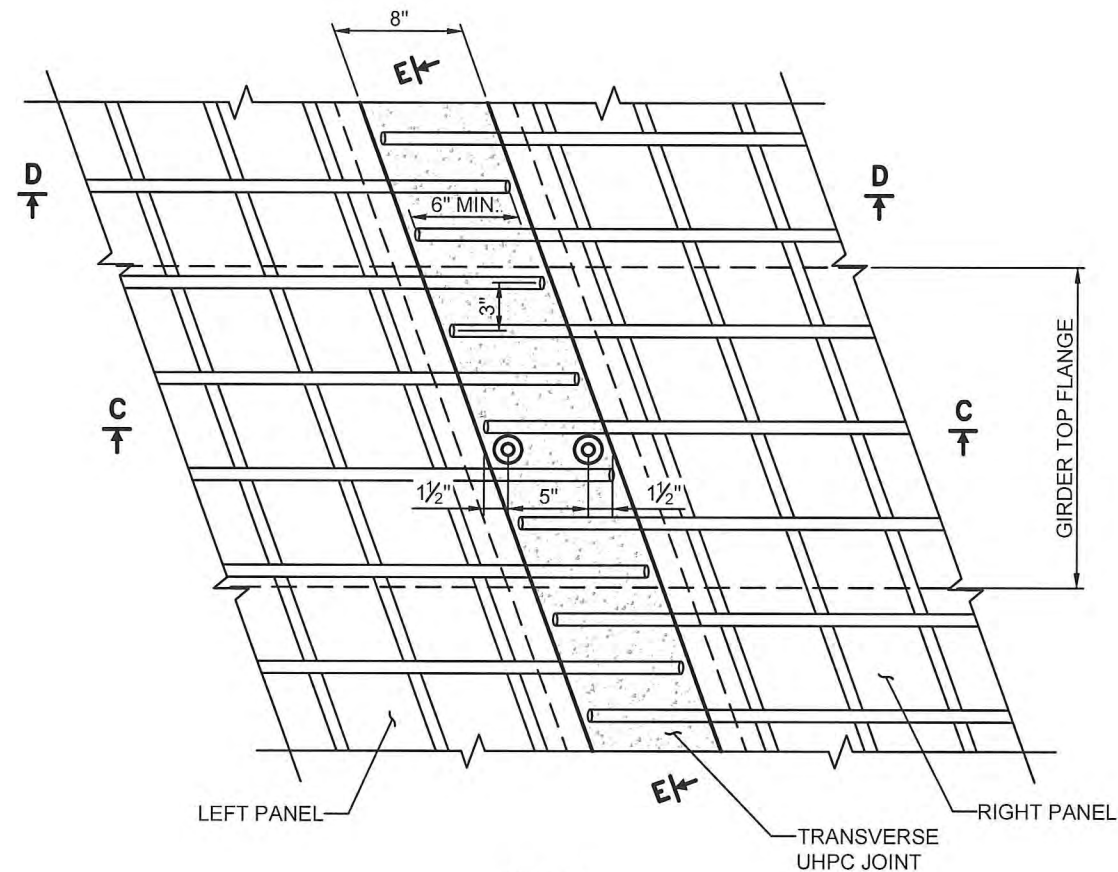
APPROVED FOR CONSTRUCTION: Kimberly Coleman DATE: 2/20/2018



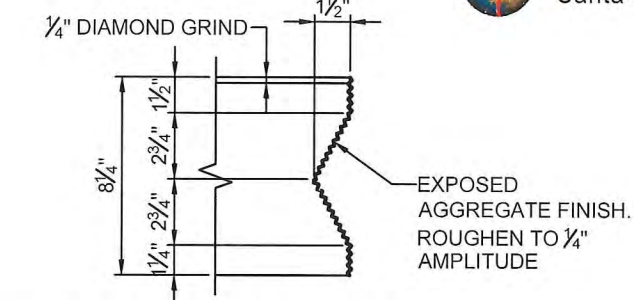
Louis Berger  
Santa Fe, New Mexico

CONTROL No.  
4101490

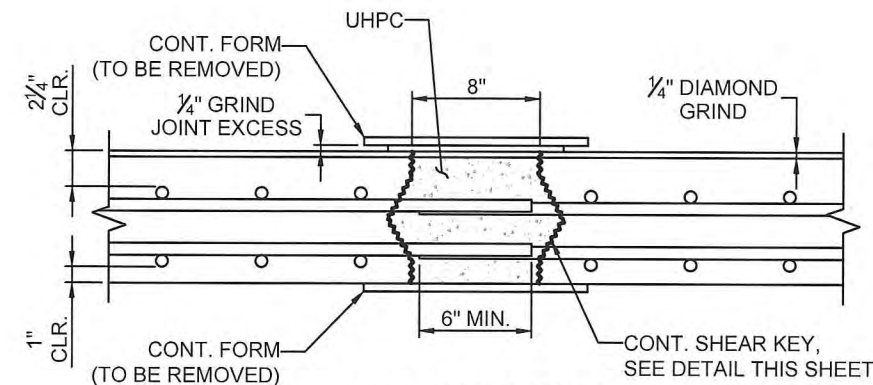
SHEET No.  
5-87



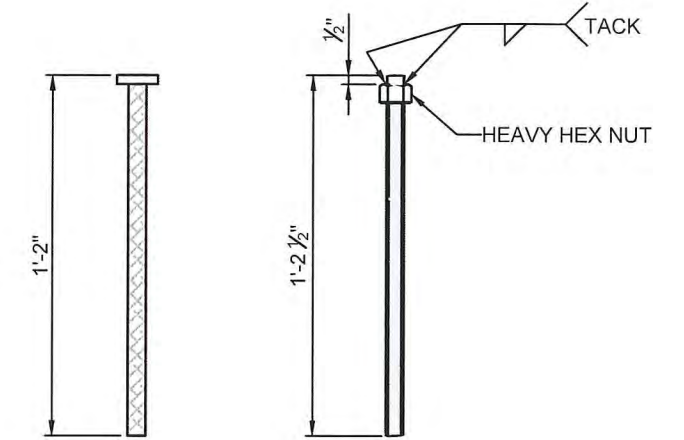
**PLAN**



**PANEL SHEAR KEY DETAIL**



**SECTION D-D**

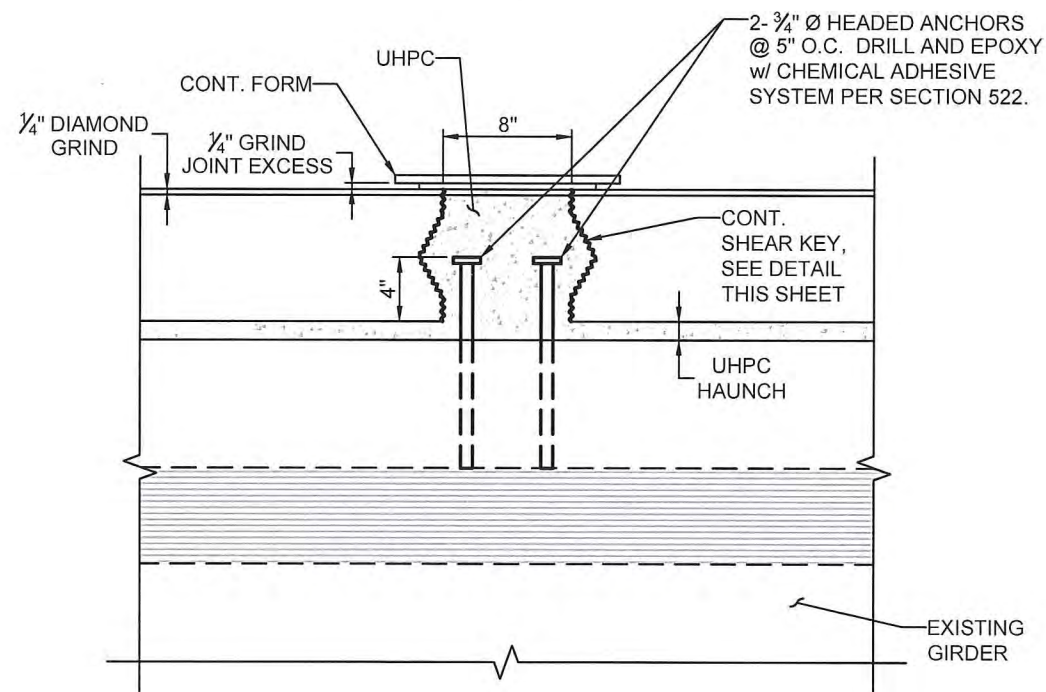


**OPTION 1**

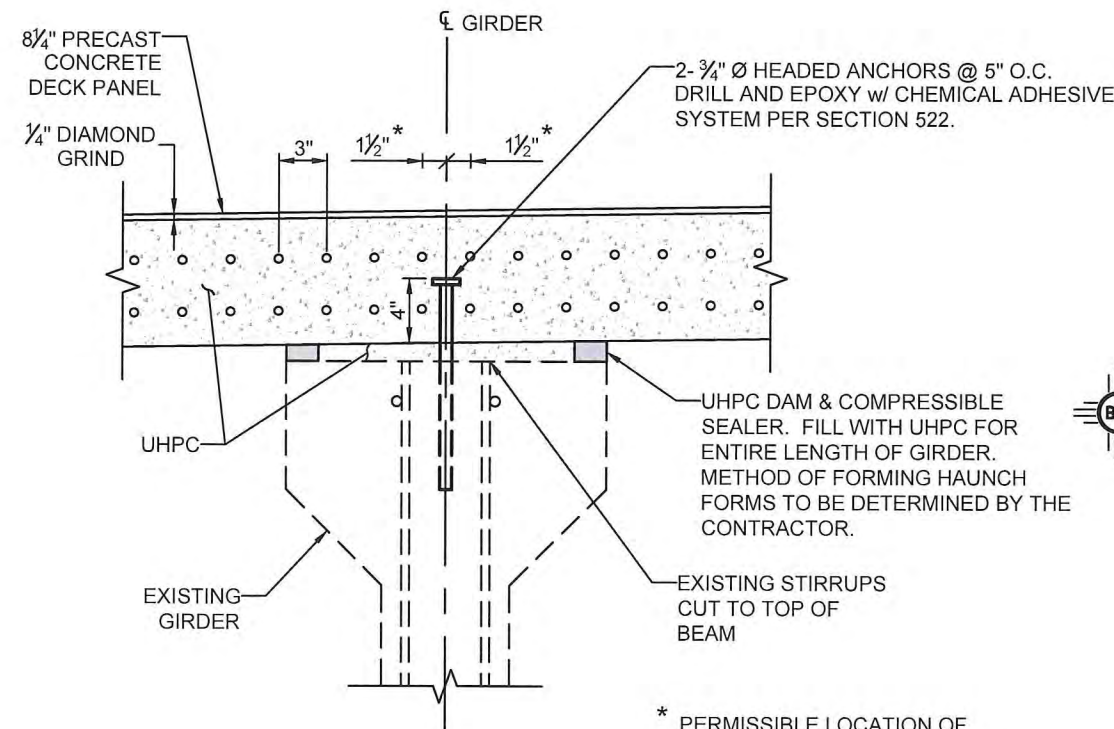
**OPTION 2**

**HEADED ANCHOR DETAIL**

(728) 3/4" x 1'-2" HEADED DEFORMED REBAR OR  
(728) 3/4" x 1'-2 1/2" THREADED RODS WITH  
WELDED HEAVY HEX NUT.



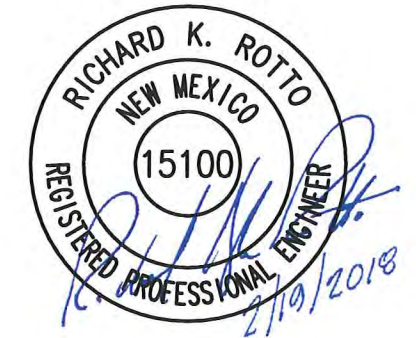
**SECTION C-C**



**SECTION E-E**

(FORMS NOT SHOWN FOR CLARITY)

\* PERMISSIBLE LOCATION OF  
SHEAR STUDS FROM  $\bar{C}$  GIRDER  
TO AVOID REINFORCEMENT



REVISIONS			
NO.	DESCRIPTION	DATE	BY
1			
2			
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NEW MEXICO DEPARTMENT OF  
TRANSPORTATION

NM-93 OVER I-40  
BRIDGE REHABILITATION

BRIDGE No. 7345

UHPC TRANSVERSE  
JOINT DETAILS

DESIGNED BY: IM DRAWN BY: CDS CHECKED BY: RKR PLOT DATE: 2/19/2018

APPROVED FOR CONSTRUCTION: Kimberly Coleman DATE: 2/20/2018



**SUGGESTED SEQUENCE OF CONSTRUCTION**

**PHASE 1 – INITIAL SET – UP**

- A. SET UP BOP AND EOP SIGNING ON NM 93 AND I-40 (SERIAL DRAWING 702-03-1/1 AND SERIAL DRAWING 702-04-1/1, SHEET 6-19 AND SHEET 6-20)
- B. INSTALL DOUBLE FINE WORK ZONE SIGNING ON I-40 (SERIAL DRAWING 702-02-1/2, SHEET 6-17)

**PHASE 2 – CONSTRUCTION OF MINOR PAVING ON WB RAMPS**

- A. CLOSE SHOULDERS ON THE WB ON/OFF RAMPS AT THE ENDEE INTERCHANGE (SERIAL DRAWING 702-10-1/3, SHEET 6-22).
- B. BUILD MINOR PAVING ON THE SHOULDERS OF THE WB ON AND OFF RAMPS. SEE SHEET 3-2 AND 3-3 FOR CONSTRUCTION LIMITS.
- C. AFTER CONSTRUCTION OF MINOR PAVING APPLY TEMPORARY STRIPING TO THE SHOULDERS AND FR - 4132. SEE SHEET 6-9 FOR TEMPORARY STRIPING PLAN
- D. INSTALL MESSAGE BOARDS, NIGHT TIME LIGHT SET UP AND TRAIL BLAZING SIGNING. SEE SHEETS 6-3 THRU 6-5.

**PHASE 3 – BRIDGE CLOSURE**

- A. INSTALL CONSTRUCTION SIGNING, TYPE III BARRICADES AND EARTH BERMS TO PERMANANTLY CLOSE BRIDGE TO COMMENCE CONSTRUCTION.

**PHASE 4 – DETOURING TRAFFIC OFF I-40 FOR BRIDGE CONSTRUCTION (SEE LIMITATIONS BELOW)**

- A. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO REDUCE I-40 EB/WB DRIVING LANE TO ONE LANE (SERIAL DRAWING 702-09-1/2, SHEET 6-21)
- B. I-40 TRAFFIC WILL BE CHANNELIZED TO ONE LANE AND WILL USE THE ON AND OFF RAMPS AS DETOURS.

**PHASE 5 – REHABILITATION OF BRIDGE No. 7345**

- A. BEGIN CONSTRUCTION OPERATIONS ON BRIDGE No. 7345. THE CONTRACTOR SHALL REMOVE CONCRETE BRIDGE APPROACH, CONCRETE BRIDGE DECK, BRIDGE RAIL AND METAL BARRIER. SEE 5-SERIES SHEETS FOR DETAILS.
- B. BUILD NEW APPROACH SLABS, INSTALL NEW CONCRETE BRIDGE DECK USING PRECAST PANELS AND PERFORM PIER AND BEARING MODIFICATIONS. SEE 5-SERIES SHEETS FOR DETAILS.
- C. INSTALL NEW BRIDGE RAIL. (SERIAL DRAWING 543-07-1/4 THRU 4/4)

**PHASE 6 – RECONSTRUCTION OF 8” CONCRETE PAVEMENT**

- A. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO CHANNEL TRAFFIC ONTO NEWLY CONSTRUCTED SHOULDER OF THE WB OFF RAMP IN ORDER FOR TRAFFIC TO ACCESS TRUCK STOP (SERIAL DRAWING 702-10-1/3, SHEET 6-22)
- B. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO CHANNEL TRAFFIC ONTO NEWLY CONSTRUCTED SHOULDER OF THE WB ON RAMP IN ORDER FOR TRAFFIC TO ACCESS I-40 (SERIAL DRAWING 702-10-1/3, SHEET 6-22)
- C. REMOVE APPROACH ROADWAY AND PERFORM RECONSTRUCTION OF 8” CONCRETE PAVEMENT ON NM-93 ROADWAY AT THE APPROACH AND DEPARTURE OF BRIDGE No. 7345.
- D. INSTALL NEW METAL BARRIER. (SERIAL DRAWING 606-GR31-1/20 THRU 20/20)
- E. THE CONTRACTOR SHALL REMOVE ALL DETOUR SIGNING FROM THE DETOUR AND REOPEN BRIDGE TO TRAFFIC.

**PHASE 7 – MISCELLANOUS CONSTRUCTION**

- A. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO REDUCE TRAFFIC OVER BRIDGE TO ONE LANE (SERIAL DRAWING 702-11-2/2, SHEET 6-23)
- B. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO REDUCE TRAFFIC TO ONE LANE ON I-40 AS NEEDED. (SERIAL DRAWING 702-09-1/2, SHEET 6-21)
- C. DIAMOND GRIND BRIDGE DECK TO PROVIDE A PLANAR SURFACE. SEE SHEET 5-8.
- D. APPLY EPOXY OVERLAY.
- E. REPAIR UNSOUND DELAMINATED AND SPALLING CONCRETE ON THE GIRDERS AND SUBSTRUCTURE.
- F. APPY EPOXY INJECTION CRACK SEALING IN BEAMS AND SUBSTRUCTURE.
- G. APPLY PENETRATING WATER REPELLENT TREATMENT.

**PLOT PHASE 8 – STRIPE ROADWAY**

- A. INSTALL CONSTRUCTION CHANNELIZATION DEVICES AND SIGNING TO REDUCE TRAFFIC OVER BRIDGE TO ONE LANE (SERIAL DRAWING 702-12-1/2, SHEET 6-24)
- B. APPLY PERMANENT STRIPING TO NM 93 ROADWAY, EB AND WB RAMPS. PERMAMNT STRIPING MUST MATCH EXISTING LA YOUT.
- C. APPLY PERMANENT STRIPING TO FR-4132 ROADWAY. PERMANENT STRIPING MUST MATCH EXIST LAYOUT.

**BRIDGE NUMBER 7345-NM-93 MP 0.10  
WORK ZONE LIMITATIONS, RESTRICTIONS & REQUIREMENTS**

- SPEED LIMIT WILL BE REDUCED TO 30 MPH IN WORK ZONE DETOUR AT ALL TIMES.
- THE CLOSURE OF I-40 IS ALLOWED ONLY DURING DAYLIGHT HOURS AS DETERMINED BY THE PROJECT MANAGER.
- CONTRACTOR SHALL ONLY DETOUR TRAFFIC ONTO RAMPS DURING DECK REMOVAL AND PLACEMENT.



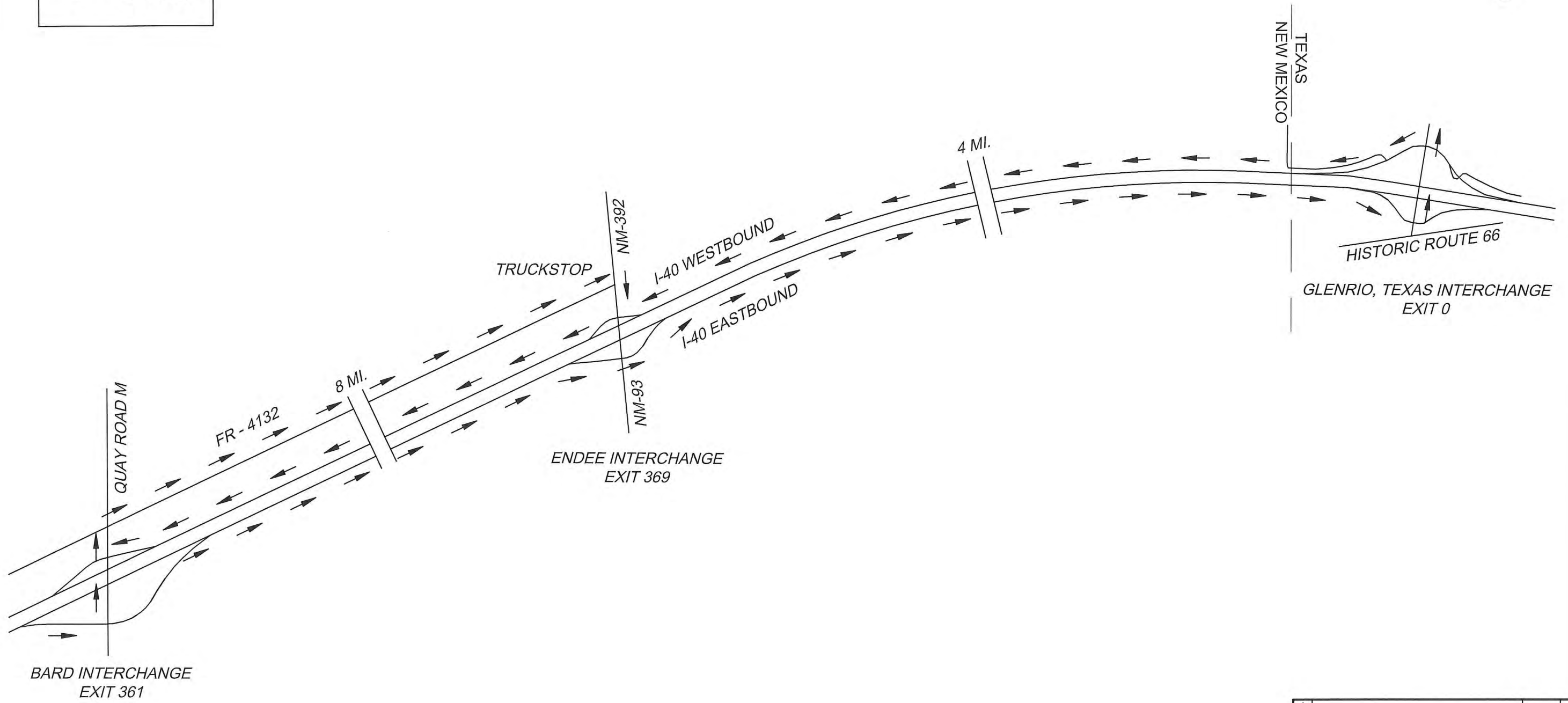
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
SEQUENCE OF CONSTRUCTION			





**LEGEND**

→ DETOUR



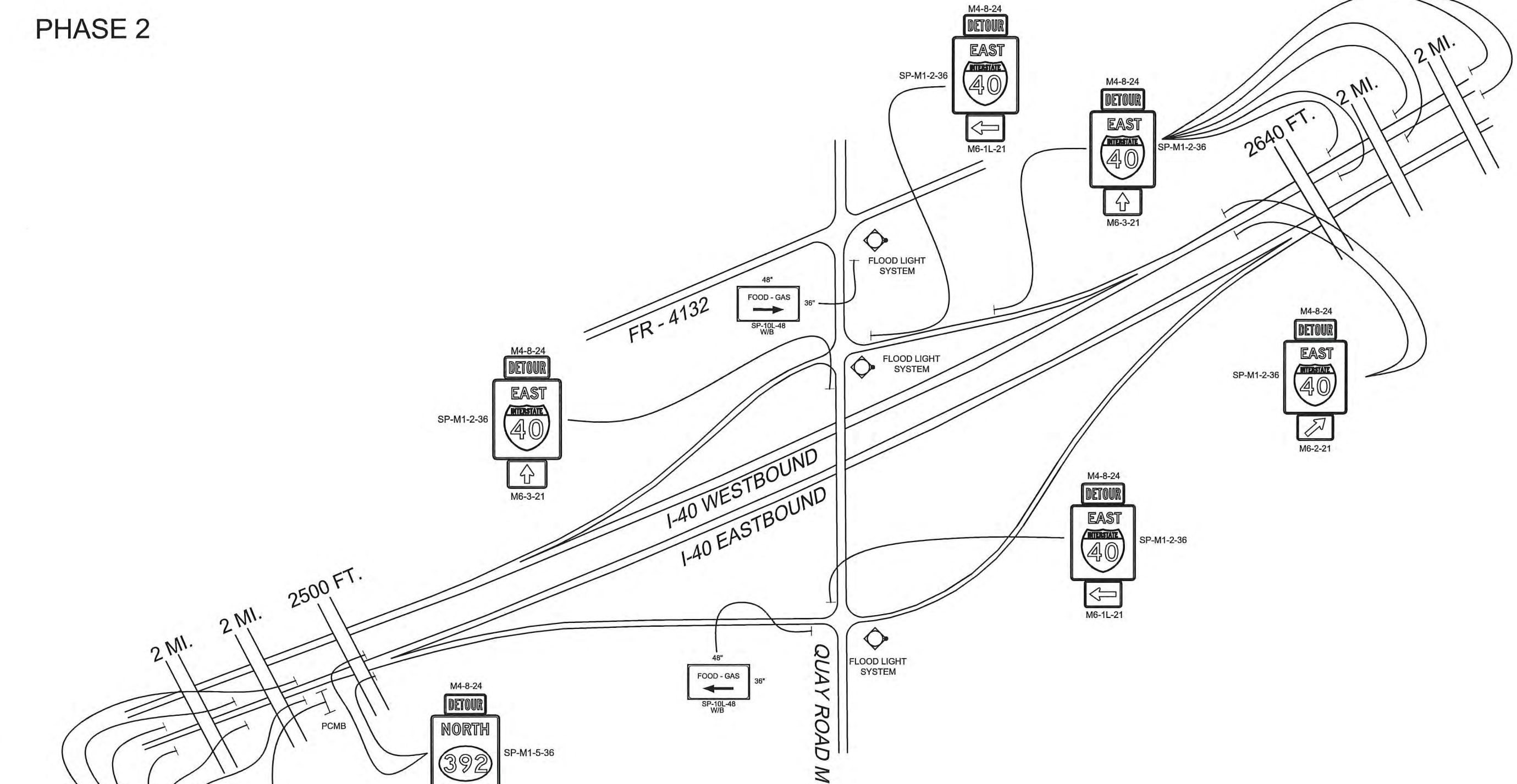
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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT  
OF TRANSPORTATION

TRAILBLAZING SIGNING

# PHASE 2



**FOOD - GAS  
USE EXIT  
361**

**EXIT 369  
CLOSED**



4			
3			
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1			
NO.	DESCRIPTION	DATE	BY

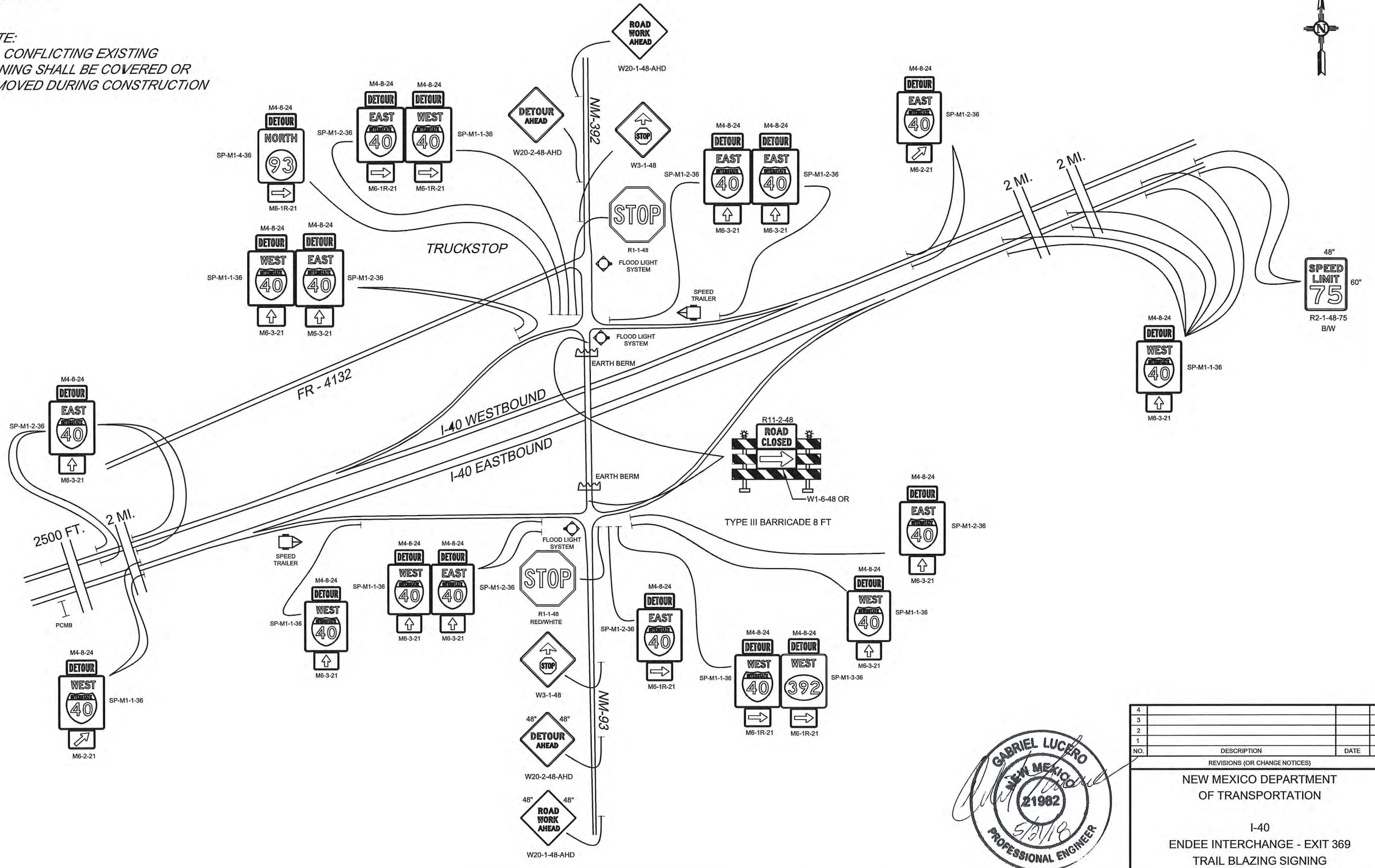
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT  
OF TRANSPORTATION

I-40  
BARD INTERCHANGE - EXIT 361  
TRAIL BLAZING SIGNING

PHASE 2

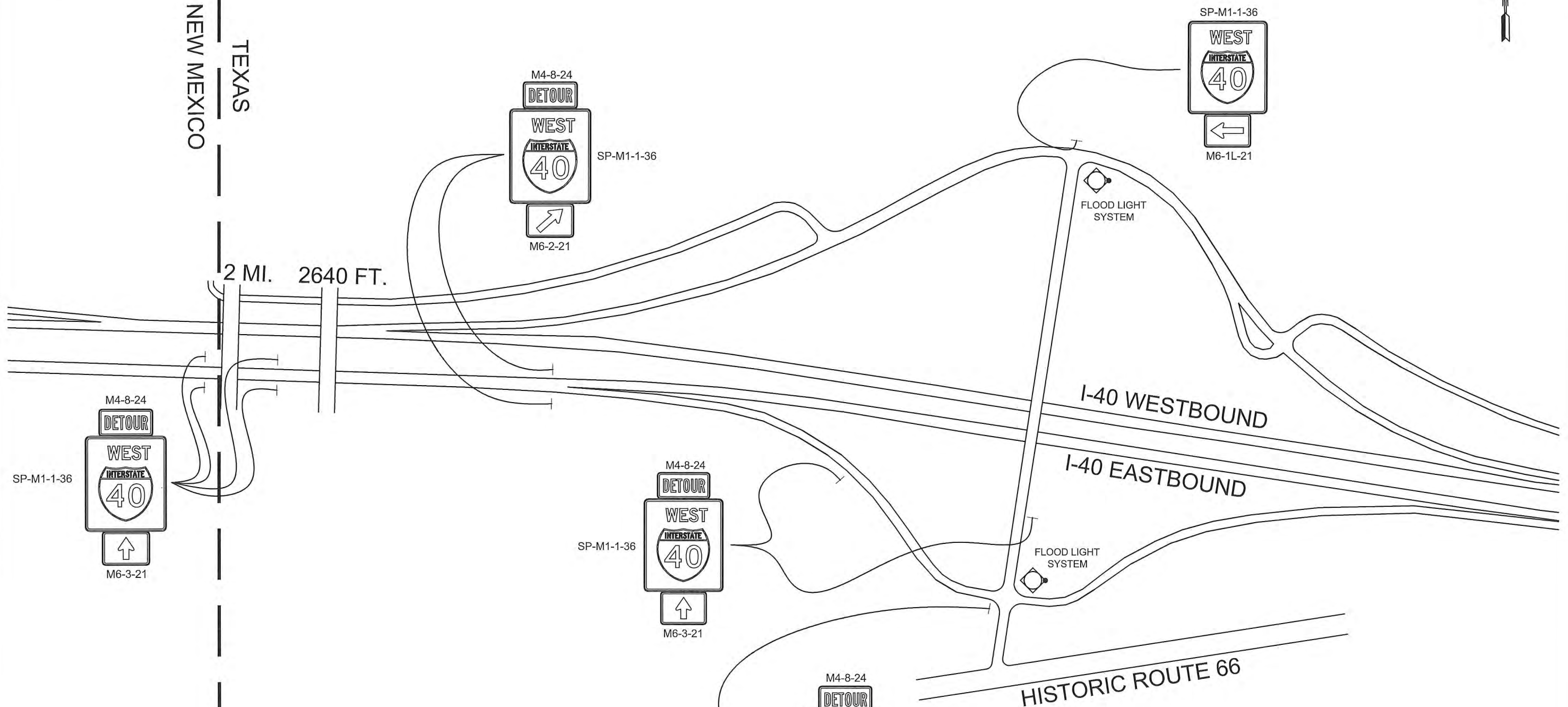
NOTE:  
ALL CONFLICTING EXISTING  
SIGNING SHALL BE COVERED OR  
REMOVED DURING CONSTRUCTION



NO.	DESCRIPTION	DATE	BY
4			
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-40 ENDEE INTERCHANGE - EXIT 369 TRAIL BLAZING SIGNING			

# PHASE 2

NEW MEXICO  
TEXAS



SP-M1-1-36  
M4-8-24  
DETOUR  
WEST  
INTERSTATE  
40  
M6-3-21

M4-8-24  
DETOUR  
WEST  
INTERSTATE  
40  
M6-2-21  
SP-M1-1-36

M4-8-24  
DETOUR  
WEST  
INTERSTATE  
40  
M6-3-21  
SP-M1-1-36

M4-8-24  
DETOUR  
WEST  
INTERSTATE  
40  
M6-1-21  
SP-M1-1-36

SP-M1-1-36  
WEST  
INTERSTATE  
40  
M6-1-21

FLOOD LIGHT SYSTEM

FLOOD LIGHT SYSTEM

I-40 WESTBOUND

I-40 EASTBOUND

HISTORIC ROUTE 66



4			
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No.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT  
OF TRANSPORTATION

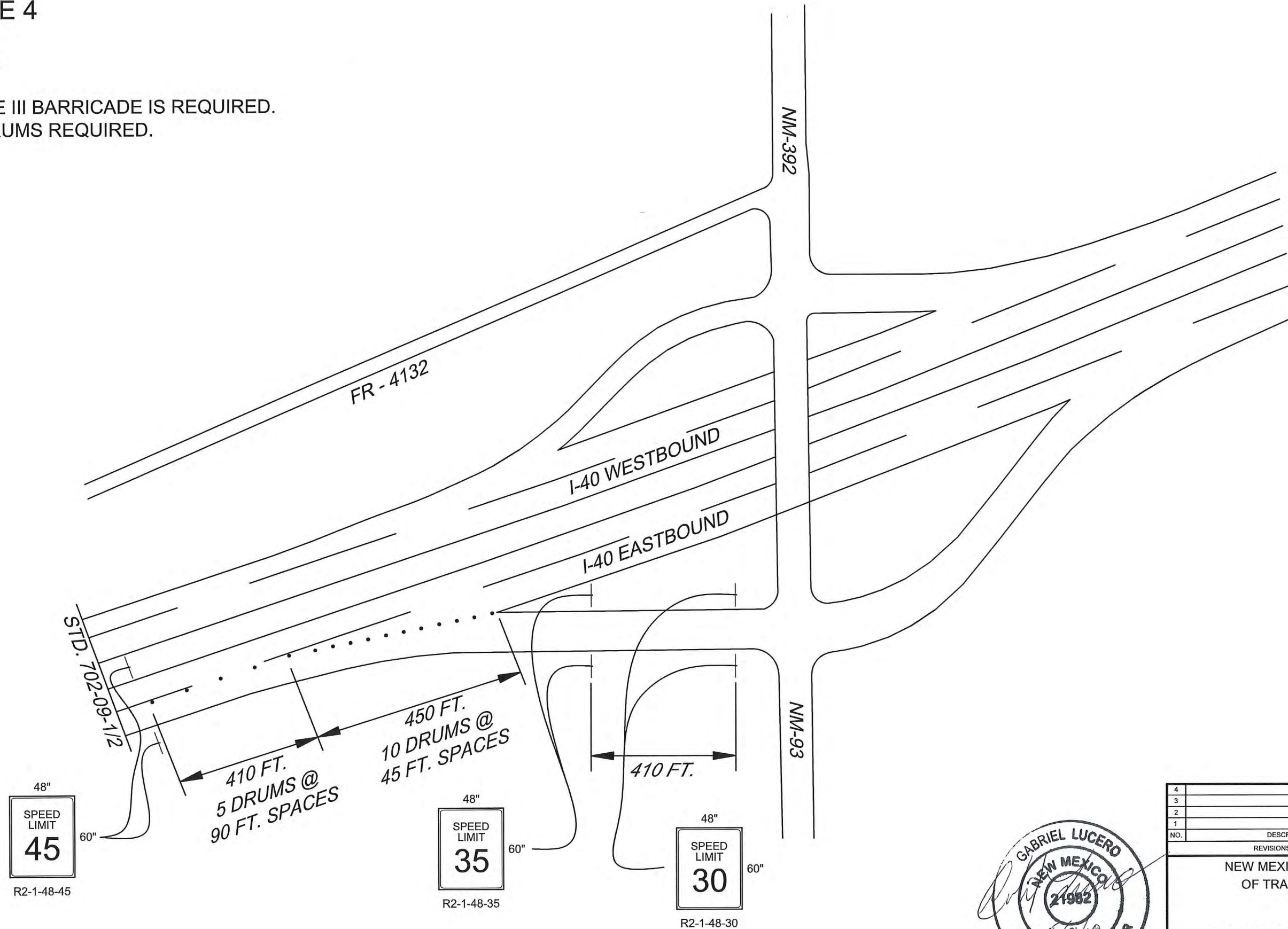
I-40  
GLENRIO, TEXAS INTERCHANGE - EXIT 0  
TRAIL BLAZING SIGNING



# PHASE 4

## NOTES:

- 1) TYPE III BARRICADE IS REQUIRED.
- 2) 8 DRUMS REQUIRED.



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NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

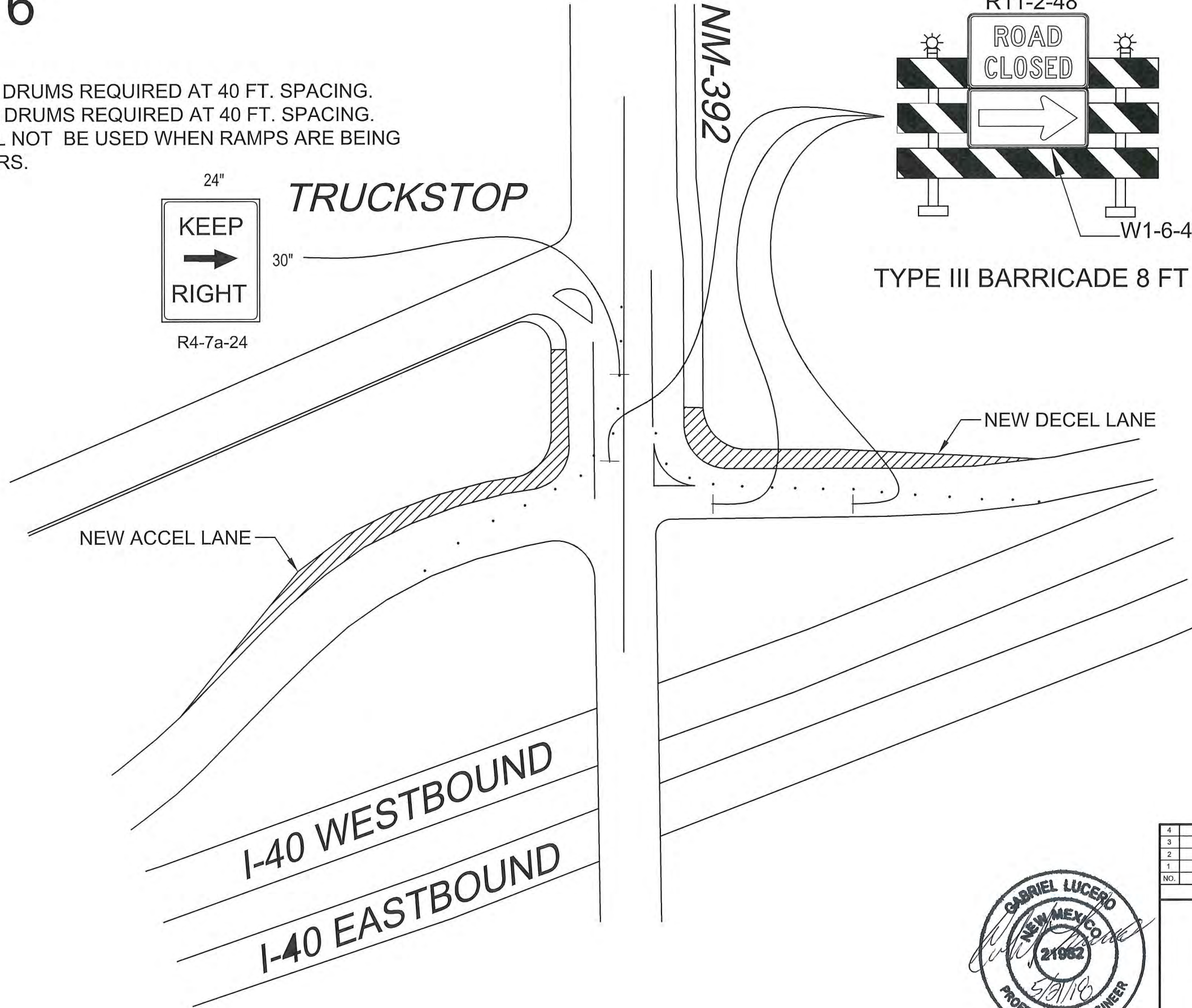
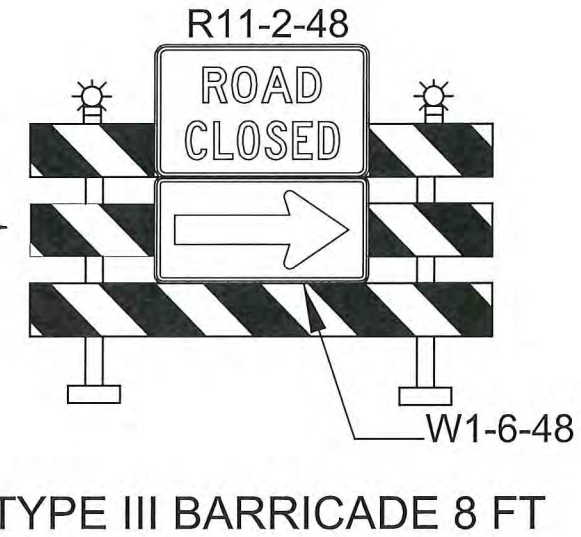
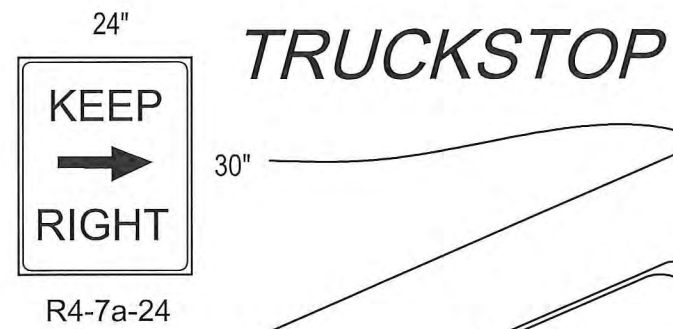
NEW MEXICO DEPARTMENT  
OF TRANSPORTATION

INTERSTATE SPEED REDUCTION

# PHASE 6

NOTES:

- 1) DECEL LANE - 15 DRUMS REQUIRED AT 40 FT. SPACING.
- 2) ACCEL LANE - 12 DRUMS REQUIRED AT 40 FT. SPACING.
- 3) THIS PLAN SHALL NOT BE USED WHEN RAMPS ARE BEING USED AS DETOURS.

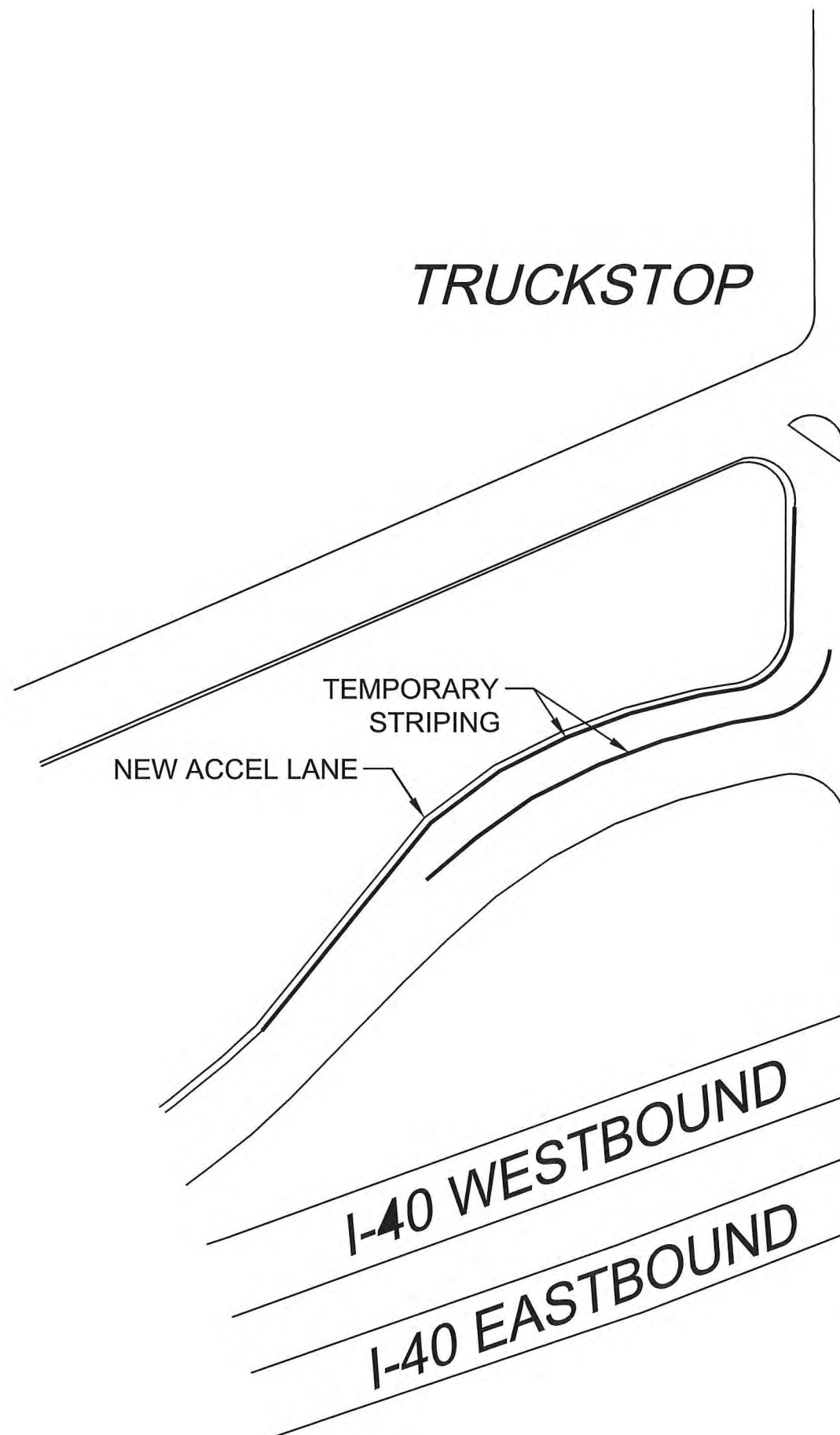


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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
NM-392 TRAFFIC CONTROL PLAN			

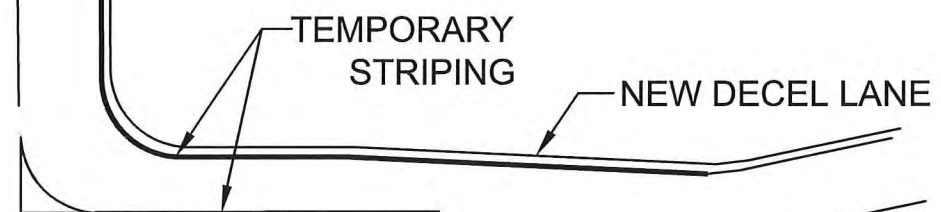
# PHASE 2



## TRUCKSTOP



## NM-392



4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

ACCEL & DECEL TEMPORARY STRIPING





ITEM NO. 702610

ITEM DESCRIPTION	UNITS	QUANTITY
PORTABLE CHANGEABLE MESSAGE SIGN	EACH	6

ITEM NO. 702612, PORTABLE FLOODLIGHT SYSTEM

STATION/LOCATION	EACH	REMARKS
BARD INTERCHANGE, EXT. 361	3.00	LIGHTS TO BE USED AT NIGHT TIME.
ENDEE INTERCHANGE, EXT. 369	3.00	
GLENRIO INTERCHANGE, EXT. 0	2.00	
<b>PROJECT TOTAL</b>	<b>8.00</b>	
<b>PROJECT USE</b>	<b>8.00</b>	

ITEM NO. 702810

TRAFFIC CONTROL DEVICES FOR CONSTRUCTION

ITEM DESCRIPTION	UNITS	QUANTITY
* CONSTRUCTION SIGNING	SQ FT	2,918
* PORTABLE SIGN SUPPORT	EACH	80
* BARRICADE TYPE III 8 FT	EACH	4
* CHANNELIZATION DEVICES TYPE DRUM	EACH	172
* SEQUENTIAL ARROW DISPLAY	EACH	3
* STEEL/BASE POST FOR CONSTRUCTION SIGNING	L.F.	3,760

\* INFORMATION FOR CONTRACTOR ONLY

ITEM NO. 702613

ITEM DESCRIPTION	UNITS	QUANTITY
SPEED DETECTOR RADAR TRAILER	EACH	2

ITEM NO. 704099, TEMPORARY STRIPING

STATION TO STATION	LENGTH FT.	LOCATION	DESCRIPTION
<b>WB OFF RAMP</b> 17+19.12 TO 21+93.12	474.00	RT. OF DECEL LANE	4" SOLID WHITE STRIPE
<b>WB ON RAMP</b> 10+38.33 TO 16+18.33	580.00	RT. OF ACCEL LANE	4" SOLID WHITE STRIPE
<b>TOTAL USE</b>	<b>1,054.00</b>		
<b>FR-4132</b>			
EB	47,520.00	RT.	4" SOLID WHITE STRIPE
WB	47,520.00	RT.	4" SOLID WHITE STRIPE
CENTER	11,880.00	C.L.	4" DASHED YELLOW CENTER STRIPE
CENTER	16,791.00	C.L.	4" SOLID YELLOW STRIPE
<b>TOTAL USE</b>	<b>123,711.00</b>		
<b>PROJECT TOTAL</b>	<b>124,765.00</b>		
<b>@ 2 APPLICATIONS</b>	<b>249,530.00</b>		
<b>PROJECT USE</b>	<b>249,530</b>		



NO.	DESCRIPTION	DATE	BY
4			
3			
2			
1			

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

CONSTRUCTION SIGNING QUANTITIES

**TRAFFIC CONTROL GENERAL NOTES:**

1. **TRAFFIC CONTROL:** All Temporary Traffic Control (TTC) devices shall be placed in accordance with the NMDOT Standard Specifications for Highway and Bridge Construction (latest edition) and the Manual on Uniform Traffic Control Devices (latest edition) and current revisions with the following constraints:
  - a. No substitutions will be allowed for channelization devices type drum unless otherwise noted in the plans.
  - b. Temporary portable sign stands are an unnecessary hazard when not in use. Unused temporary sign stands shall be removed from the roadway. If temporary sign stands are staged for future use (approved by the project manager) they shall be folded up and stored away from the paved shoulder.
  - c. Use of Type I or II barricades on roadways with speed limit greater than 40 mph is strictly prohibited.
  - d. The work zone shall comply with, but not limited to, NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control.

2. **BOP AND EOP SIGNING:** BOP and EOP signing in accordance with Standard Drawing 702-03-1/1 and/or 702-04-1/1 shall be placed at the project limits prior to construction operations commencing and shall remain in place throughout the duration of the project or as directed by the Project Manager. Advance warning signs shall be placed at all side streets.

3. **FLAGGING:** Flagging shall be provided for safety per the plan or as directed by the project manager and shall conform to the MUTCD latest edition. The flaggers, applicable signs and other related items shall be considered incidental to the completion of the project and no separate measurement or payment will be made.
  - a. All flaggers shall be certified and shall have their certification available for review at all times when on duty.
  - b. Flagging operations shall adhere to NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control; Flaggers shall wear high-visibility safety apparel that meets Performance Class 2 or 3.

4. **INGRESS AND EGRESS:** The Contractor shall provide ingress and egress to local residences and businesses for the duration of the project. If access closure is required, the contractor shall request the closure through the Project Manager. Upon approval, the Contractor shall coordinate such closure with the property owners and the Project Manager at least 48 hours in advance. All work associated with this shall be considered incidental to the completion of the project and no separate payment or measurement will be made.

5. **PORTABLE CHANGEABLE MESSAGE SIGNS:** The Contractor shall supply Portable Changeable Message Signs (PCMS), which will be retained by the contractor. The message PCMS shall be utilized to convey messages, expected delays, and detours to motorists as required. Messages should be determined by the Contractor and approved by the Project Manager. Two (2) Weeks prior to start of construction: name locations, and number of PCMS to be placed.

6. **TRAFFIC CONTROL PLANS:** This traffic control plan (TCP) represents a suggested method for traffic control during construction. Adjustments to the details of this TCP and requirements within the plan may be necessary due to construction activities, or as directed by the Project Manager. If the contractor elects to make any changes to the TCP or sequence of construction, the Contractor shall submit four (4) 11" X 17" copies of the proposed TCP to the Project Manager at least two (2) weeks prior to implementation. The TCP shall conform to the current editions of the MUTCD, NMDOT Standard Specifications and AASHTO Roadside Design Guide. The TCP shall be in computer drafted format and shall be designed, stamped, and revised as necessary by a current New Mexico Licensed professional engineer and submitted to the Project Manager for approval. All costs associated with developing the TCP and any additional devices associated with the TCP shall be incidental to Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.

7. **PUBLIC INFORMATION:** The Contractor / TCP firm shall contact the Project Manager or District Public Information Officer, as preferred by the district through the district office, to confirm the actual start dates of the construction and the contractor's schedule a minimum of 48 hours before any work listed in the TCP is performed.

8. **REMOVAL OF CONSTRUCTION SIGNING:** All temporary traffic control signs, sign posts and post bases installed with the construction project shall be REMOVED by the Contractor at the completion of the project. Removal shall consist of complete extraction of the bases from the ground. This work shall be incidental to the completion of the project and no separate measurement or payment will be made.

9. **CONFLICTING SIGNS:** All conflicting signs within or in advance of the work zone shall be covered completely with an opaque non-light transmitting material so as not to damage the sign. The Contractor is to use an approved method of covering existing signs so as not to damage/distort the sign sheeting or markings. The Contractor shall not place tape directly on the face of the sign. Failure to adhere to this requirement will result in the Contractor replacing the sign at no cost to the NMDOT.

10. **TEMPORARY STRIPING:** The use of black paint to cover existing lane lines or symbols is strictly prohibited. All temporary striping shall be placed before opening any work zone or portion of a work zone in accordance with the MUTCD and the approved traffic control plan. This work shall be included in Item No. 704100, "Removable Marking Tape" and no separate measurement or payment will be made, unless otherwise noted in the contract.

11. **CONSTRUCTION SIGNING:** All construction signing shall meet retroreflectivity requirements listed in section 702.2.1 "Construction Signing" of the NMDOT Standard Specifications.

- a. All construction signing on the interstate and on high speed (greater or equal to 45 MPH) multilane divided facilities shall be double indicated (left and right shoulders).
- b. All signs that are part of work zone that are in place for more than 3 days shall be placed on breakaway posts. If there are physical restrictions at the site that prohibit the sign from being placed on posts, the Contractor shall notify the District Traffic Engineer and obtain a waiver.
- c. All warning and regulatory signs shall meet the following size requirements:
 

Interstate:	Warning sign 48"x48"	Regulatory 48"x60"
Non-Interstate:	Warning sign 36"x36"	Regulatory 36"x42"
- d. The following reflectivity material shall be used on all construction signing placed on NMDOT roadways.

SIGN	SIGN CODE	COLOR	LETTER SHEETING	BACKGROUND SHEETING
APPROACH SIGNS	W20-XX	BLK/FLUORESCENT ORANGE	----	TYPE VIII, IX, XI
CHEVRONS	W1-8	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI
CURVES	W1-2	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI
REVERSE CURVE	W1-4	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI
MERGE	W4-1	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI
NO PASSING ZONE	W14-3	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI
FLAGGER PADDLE	----	BLK/FLUORESCENT ORANGE on Side 1 with RED on Side 2	----->	TYPE VIII, IX, XI Type IV White
ALL DRUMS	----->	WHITE/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI Type IV white
All Other Const. Signs	----->	BLK/FLUORESCENT ORANGE	----->	TYPE VIII, IX, XI

12. **REMOVAL OF TEMPORARY STRIPING:** All relevant temporary striping shall be removed upon completion of each phase of construction. The only approved method of stripe removal is water blasting. The contractor is to ensure that there is no conflicting striping through the work zone or through detours. This work shall be included in Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.
13. **SATISFACTORY WORKING CONDITION:** All traffic control devices used on this project shall be in satisfactory working condition and shall function equivalently to new equipment in accordance with the MUTCD (latest edition). Traffic Control work zone shall comply with requirements of FHWA 23CFR 630 Subpart K for traffic control devices. At the beginning of the project 100% of signs/devices shall be in acceptable condition (new or like new). After 2 weeks at no time shall less than 75% of devices be in acceptable condition. All traffic devices shall be kept clean throughout the duration of the project. Any sign that is tagged by graffiti shall be cleaned (as long as it does not affect the reflective sheeting) within 24 hours or removed and replaced.
14. **TRAFFIC CONTROL FIELD ADJUSTMENTS:** Location of device spacing shall be field verified to account for existing roadway features which may obstruct placement and/or view of devices. Any changes to the traffic control plan should be approved by the District Traffic Engineer or his/her designee. All field adjustments of signs should be approved by the District Traffic Engineer. This work shall be considered incidental to Item No. 618000 "Traffic Control Management" and no additional measurement or payment will be made, unless noted in the contract.
15. **PEDESTRIANS:** Routes/paths shall not be closed without providing a detour. ADA requirements shall be adhered to; ADA compliant devices shall be used for channelization.
16. **BICYCLES:** Shall be accommodated or rerouted per MUTCD recommendations.

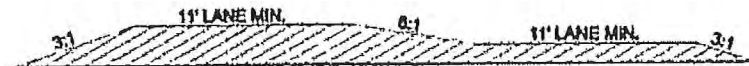
3/2/2017	DRB	REVISE NOTE 2	
3/2/2017	DRB	REVISE NOTE 7	
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC CONTROL GENERAL NOTES			
APPROVED:	Afshin Jafar		3/2/17
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
702-01-1/5			

**TRAFFIC CONTROL GENERAL NOTES (CONTINUED):**

17. **TEMPORARY CONCRETE WALL BARRIER (CWB):** When flaring the leading end of a Temporary Wall Barrier (CWB) within a construction work zone, the flare rate shall be done in accordance with the rates shown in the table below: (NMDOT Standard Drawing 606-20-5/5)

Roadway Speed Limit	Minimum Taper/ Flare Rate	Desirable Taper/ Flare Rate
Less than 45 MPH	8:1	18:1
Between 45 MPH and 55 MPH	10:1	24:1
Greater than 55 MPH	15:1	30:1

- a. When temporary wall barrier is placed in a construction work zone, a 5' clear area is required between the CWB and the work zone to accommodate barrier deflection. When a 5' clear area is not attainable, CWB shall be anchored to the pavement surface.
- b. Temporary CWB shall be provided with reflective barrier delineators as indicated in NMDOT standard drawing 606-21-1/1.
18. **CRASH ATTENUATORS:** The crash cushion attenuators shall be designed as per the District Traffic Engineer's recommendations. The District may elect to either utilize the pre-construction posted speed, or the 85% speed in the layout of the crash cushion attenuators within the work zone.
19. **DROP OFF POLICY:** In the areas of pavement operations or other activities within the traveled way and adjacent to the existing traveled lane, the contractor shall assure that no pavement drop-offs are left exposed during non-working hours. The contractor shall initiate corrective means as per "the New Mexico Department of Transportation Pavement Drop-off Guideline" to achieve a minimum 6:1 slope between traveled lanes and a minimum 3:1 slope adjacent to the existing traveled lane with two 11foot driving lanes as shown in the detail below. (AD241)



20. **Lane Closures:** The Contractor/TCP firm shall not place a lane closure taper along a horizontal curve. The taper shall be placed in advance of the horizontal curve so that it is visible to all oncoming traffic. On crest vertical curves, the Contractor/TCP firm shall place lane closures in advance of, or at the beginning of the curve to enhance visibility of the lane closure to oncoming traffic.
21. **Sequential Arrow Display:** Placement of the sequential arrow shall be at or near the beginning of the lane closure taper. In areas of insufficient pavement width, the sequential arrow may be placed within the taper, but not to exceed 1/2 the taper length. In all cases, the sequential arrow shall be placed behind the channelization devices. The shoulder shall be closed in advance of the merging taper to direct vehicular traffic to remain within the traveled way. (MUTCD 6F.61)
22. **ADDITIONAL SIGNS:** "BUMP", "LOOSE GRAVEL", "LANE DROP-OFF SIGN" sign placement: The contractor shall place WB-1 sign ("BUMP" - B/FO), WB-7 sign ("LOOSE GRAVEL" - B/FO) and/or WB-17 signs ("SHOULDER DROP-OFF" - B/FO) in advance of bridge approaches or other locations during cold milling and overlay operations as needed or as directed by the project manager.
23. **CLEAR ZONE:** All stationary objects within clear zone shall be properly shielded and outlined with drums mounted with Type "A" warning lights. Use of vertically mounted retro-reflective material in lieu of a Type A warning light is strictly prohibited.
- a. Equipment, materials, or vehicles stored within Right-of-way (ROW) shall be outside of clear zone (based on existing posted speed).
- b. Equipment, material or vehicles stored within clear zone shall be properly shielded.
- c. Materials, work activities, equipment, and vehicles shall not be stored within the established buffer space of the project work zone.
- d. All construction equipment, vehicles and materials shall remain behind traffic control devices.
24. **TRAFFIC CONTROL MANAGEMENT:** The contractor or the traffic Control Subcontractor shall provide a Traffic Control Supervisor on site during working hours for response within 1 hour to traffic control issues/concerns.

25. **INCIDENT MANAGEMENT:** Contractor is required to comply with requirements of FHWA CFR 630 Subpart J for Work Zone Safety and Mobility which shall include an Incident Management Plan to be utilized for the entire duration of the project. The Incident Management Plan shall contain a method to address traffic flow through the work zone during incidents. The Incident Management Plan must be reviewed and approved by the District Traffic Engineer. The plan shall contain the following as a minimum:

- a. Contacts for the contractor, local enforcement, safety agencies, municipal agencies, public information officer and NMDOT
- b. Steps to be followed during incidents
- c. Method of recording and reporting incidents

26. **LIST OF INCIDENTALS - No Additional Payment Associated**  
 LIST OF INCIDENTALS for Temporary Traffic Control/ MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

	LIST OF INCIDENTALS for Temporary Traffic Control
A.	MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC CONTROL GENERAL NOTES			
APPROVED:	<i>Afsin J...</i>		DATE: 12/10/15
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
702-01-2/5			

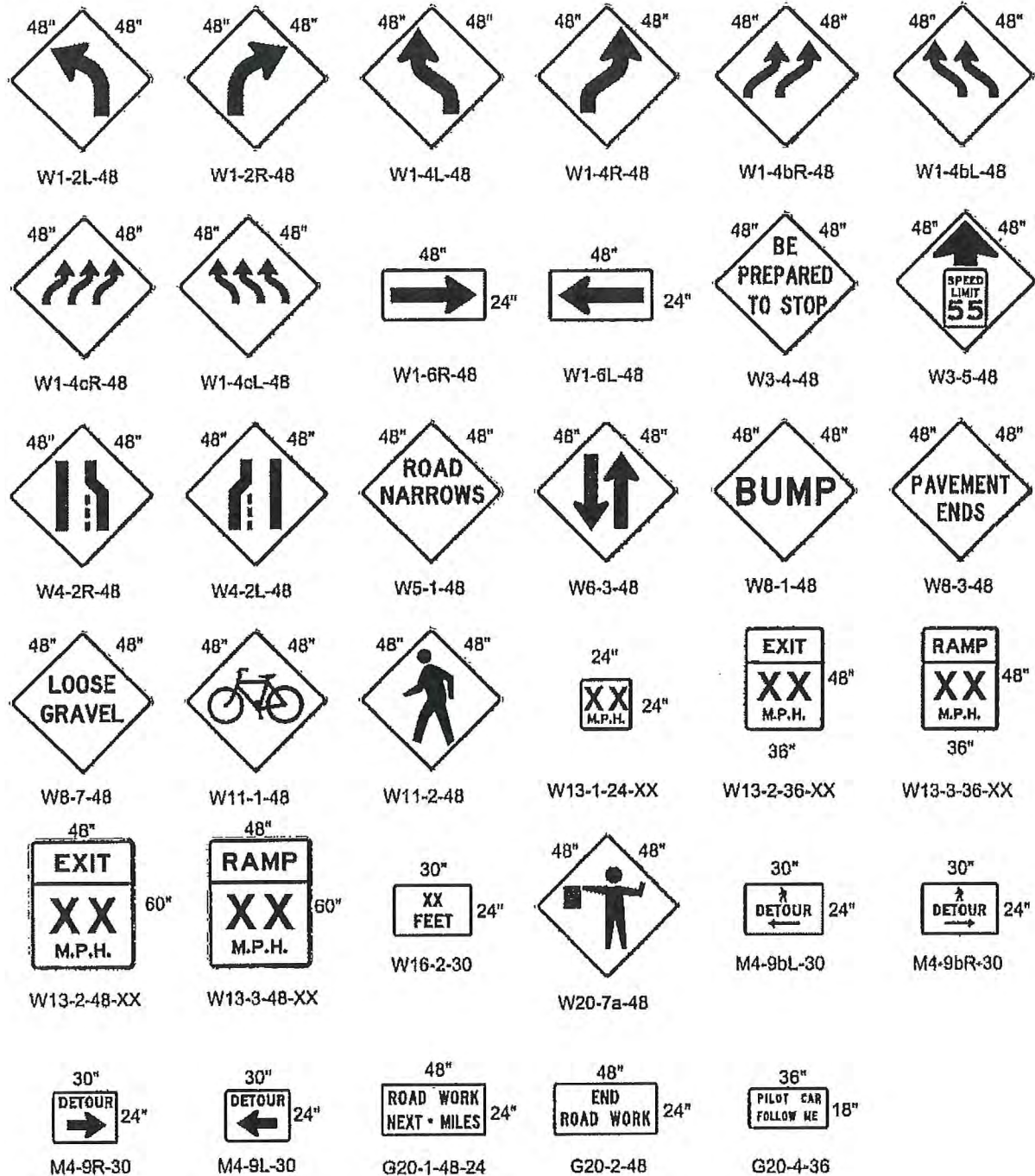
**SIGN FACE DETAILS**

**FOR CONSTRUCTION / MAINTENANCE**

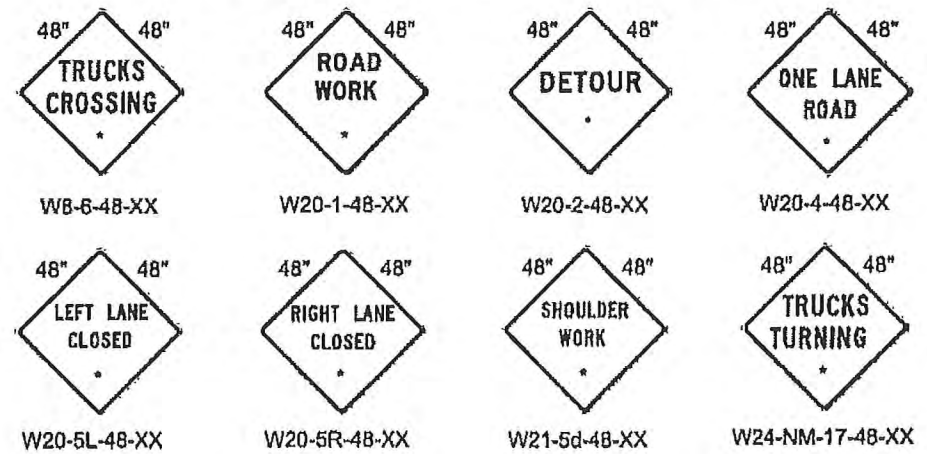
1. SIGNS SHALL MEET SPECIFICATIONS IN THE STANDARD HIGHWAY SIGNS MANUAL AND CURRENT EDITION OF THE MUTCD.
2. SEE CURRENT EDITION OF MUTCD FOR ADDITIONAL SIGNS.
3. ALL SIGNS SHALL COMPLY WITH SHEETING REQUIREMENTS AS SPECIFIED IN STANDARD DRAWING 702-01-3/3.
4. SIGN SIZES MAY BE ADJUSTED PER MUTCD RECOMMENDATIONS.

**WARNING SIGNS:**

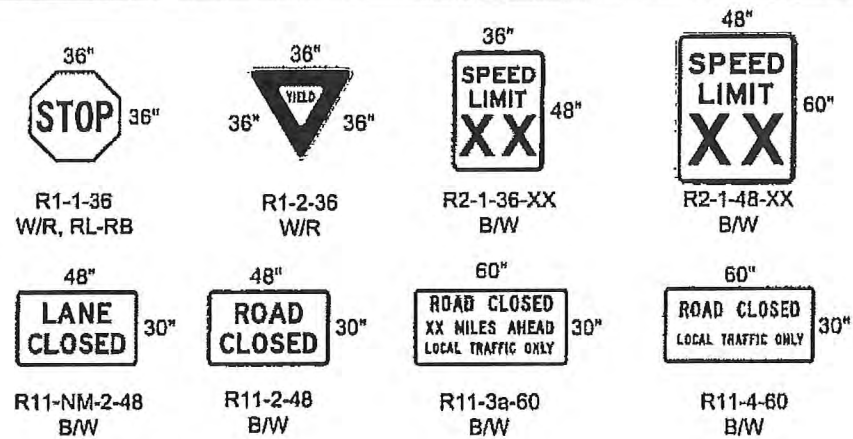
ALL WARNING SIGNS ARE BLACK/FLUORESCENT ORANGE UNLESS OTHERWISE SPECIFIED.



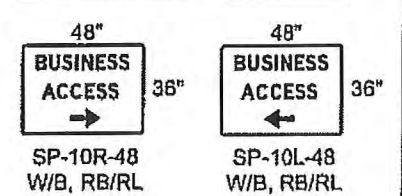
\* THESE SIGNS REQUIRE APPROPRIATE DISTANCE INDICATION  
(1/2 MILE, 1 MILE, 1500 FT., 750 FT., 500 FT., 350 FT.)



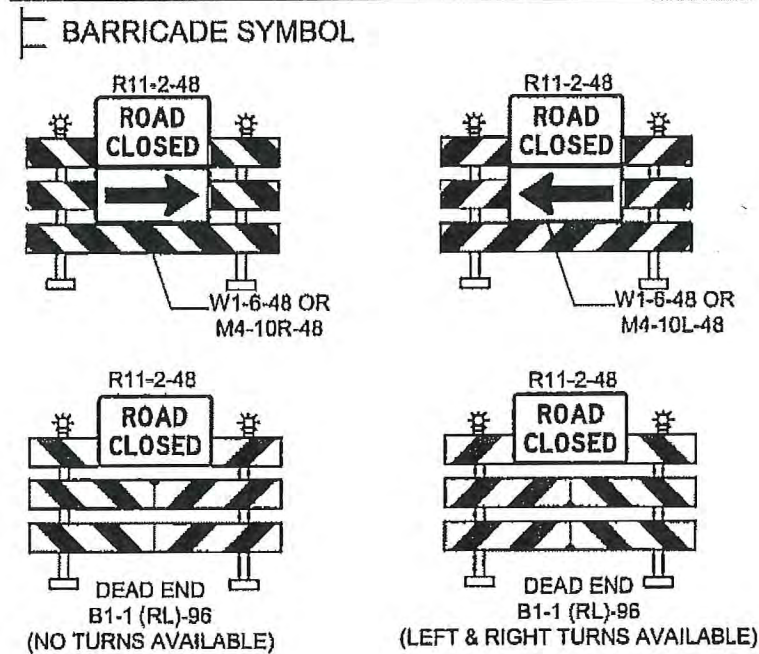
**REGULATORY SIGNS:**



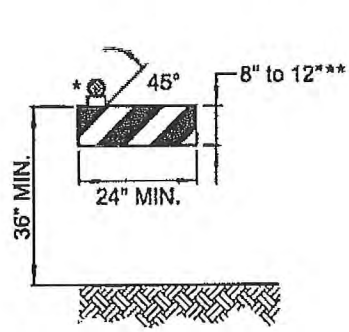
**SPECIAL SIGNS:**



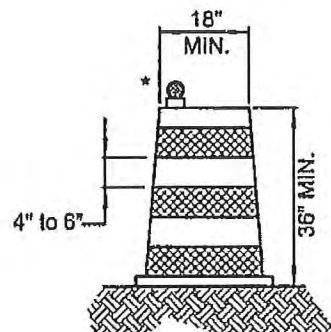
**8 FT. TYPE III BARRICADES**



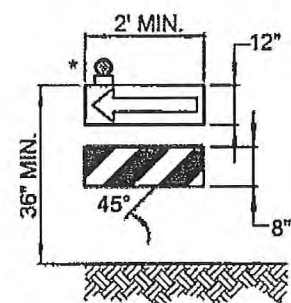
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
CONSTRUCTION & MAINTENANCE SIGN FACE DETAILS			
APPROVED:	[Signature]		12/10/15
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
702-01-3/5			



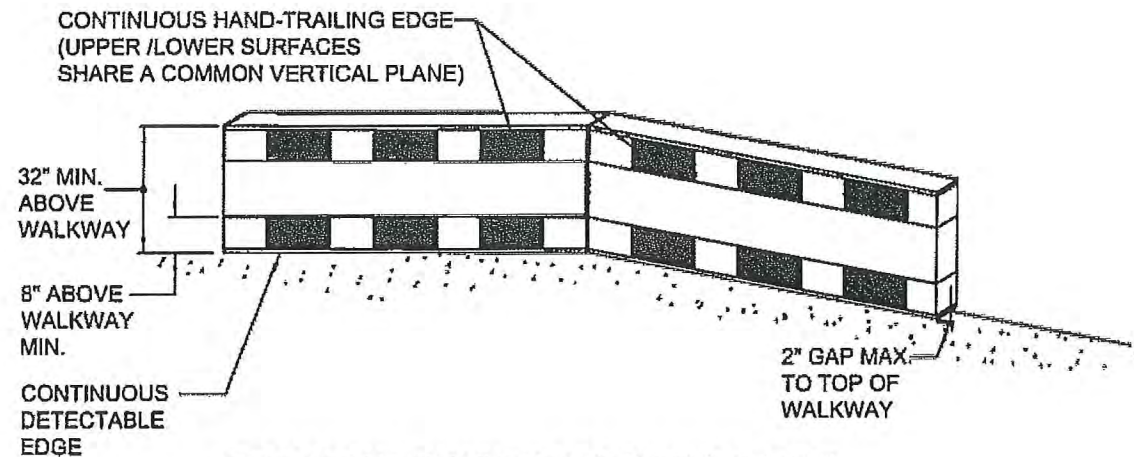
**TYPE I BARRICADE** \*\*  
LOW SPEED ROADWAY ( $\leq 40$  MPH)



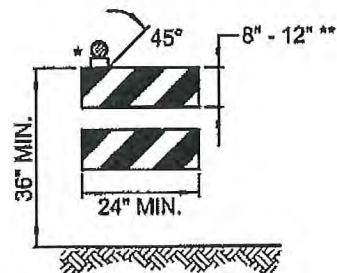
**DRUM**\*



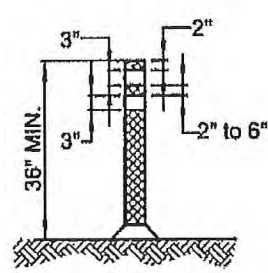
**DIRECTION INDICATOR BARRICADE** \*\*



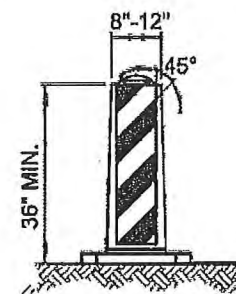
**PEDESTRIAN CHANNELIZATION DEVICE (PCD)**



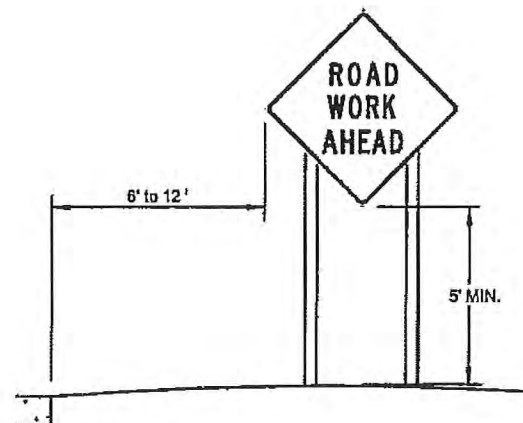
**TYPE II BARRICADE** \*\*\*  
LOW SPEED ROADWAY ( $\leq 40$  MPH)



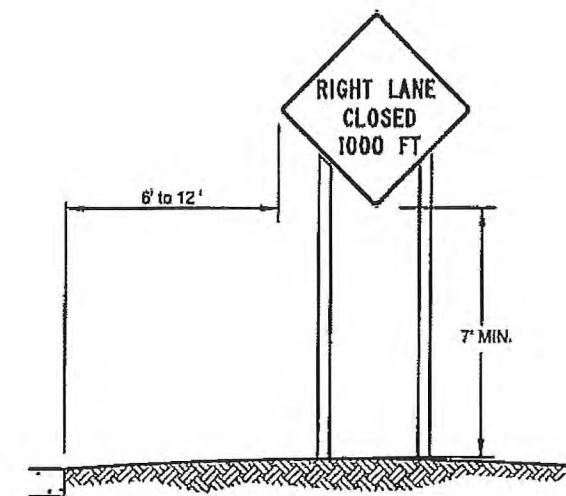
**TUBULAR MARKERS**



**VERTICAL PANEL** \*\*\*

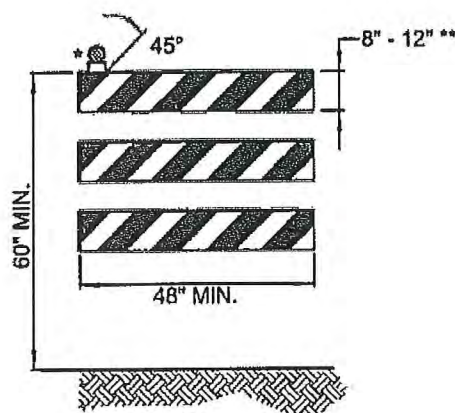


**RURAL AREA**

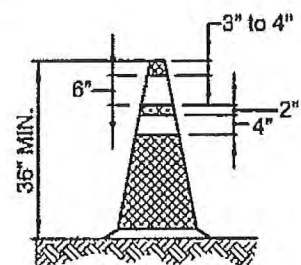


**BUSINESS, COMMERCIAL, OR RESIDENTIAL AREA (WITHOUT CURB)**

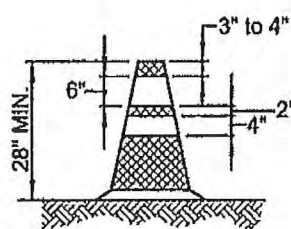
**HEIGHT AND LATERAL LOCATIONS OF SIGNS - TYPICAL INSTALLATIONS**



**TYPE III BARRICADE** \*\*



**NIGHT AND/OR FREEWAY HIGH SPEED ROADWAY ( $\geq 45$  MPH)**



**DAY AND LOW SPEED ROADWAY ( $\leq 40$  MPH)**

**CONES**

**NOTE:**  
ALL DEVICES USED ON NMDOT ROADWAYS SHALL BE ON THE APPROVED PRODUCTS LIST.

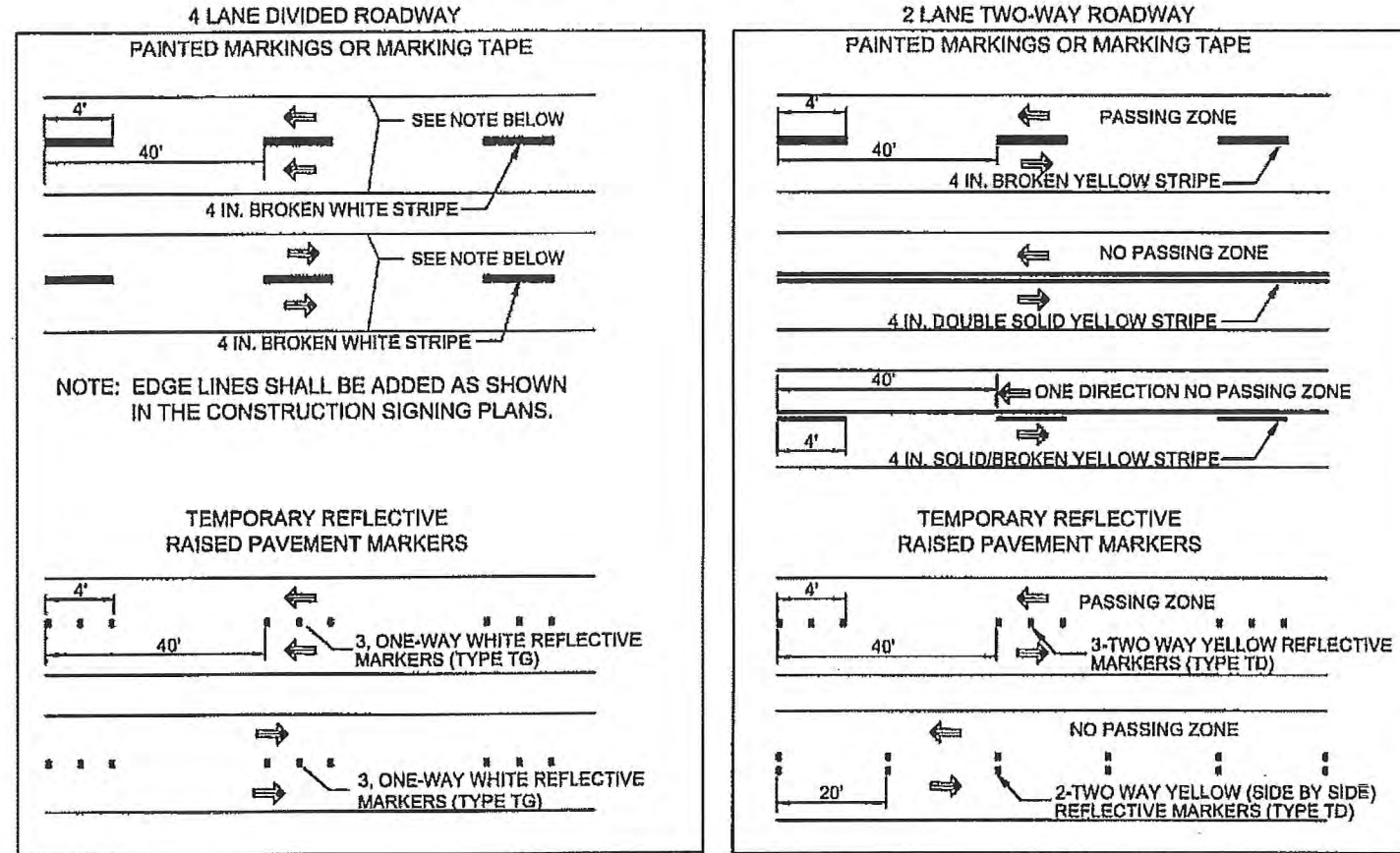
- \* WARNING LIGHT (OPTIONAL)
- \*\* NOMINAL LUMBER DIMENSIONS ARE SATISFACTORY FOR BARRICADE RAIL WIDTH DIMENSIONS.
- \*\*\* RAIL STRIPE WIDTHS SHALL BE 6 INCHES. EXCEPTION: WHERE RAIL LENGTHS ARE LESS THAN 36 INCHES. THEN 4 INCH WIDE STRIPES MAY BE USED.

THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES. VERTICAL PANELS AND BARRICADES USED ON HIGH-SPEED ROADWAYS, EXPRESSWAYS, AND FREEWAYS SHALL HAVE A MINIMUM OF 270 SQUARE INCHES OF RETROREFLECTIVE AREA FACING TRAFFIC.

DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING		
CHANNELIZATION DEVICES FOR CONSTRUCTION, MAINTENANCE, UTILITY & INCIDENT MANAGEMENT OPERATIONS		
APPROVED:	<i>A. Shins</i>	11/3/15
DESIGNED BY:	DRAWN BY:	CHECKED BY:
702-01-4/5		

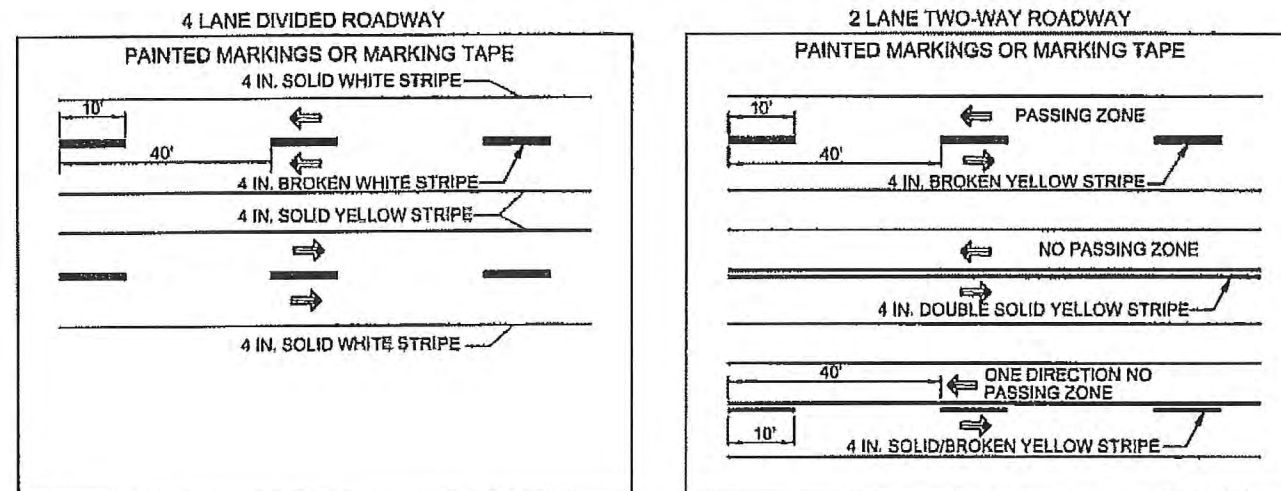
**FIGURE 1**

SHORT TERM WORK ZONE INTERIM MARKINGS (IN PLACE FOR LESS THAN 14 CALENDAR DAYS) (MINIMUM OF 2 COATS OR AS DIRECTED BY THE PROJECT MANAGER)



**FIGURE 1A**

STANDARD WORK ZONE INTERIM MARKINGS (IN PLACE FOR 14 CALENDAR DAYS OR MORE) (MINIMUM OF 2 COATS OR AS DIRECTED BY THE PROJECT MANAGER)

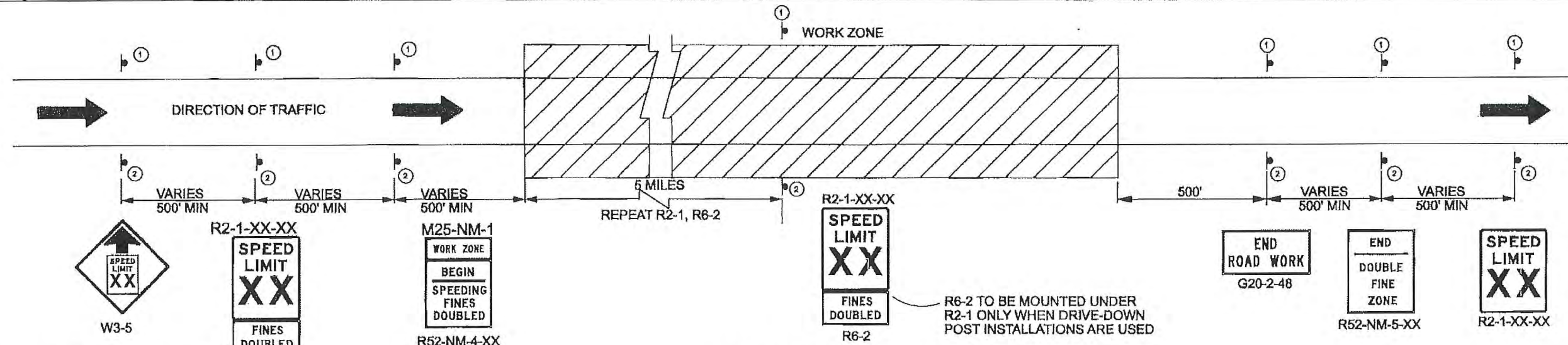


**GENERAL NOTES**

**WORK ZONE INTERIM MARKINGS:**

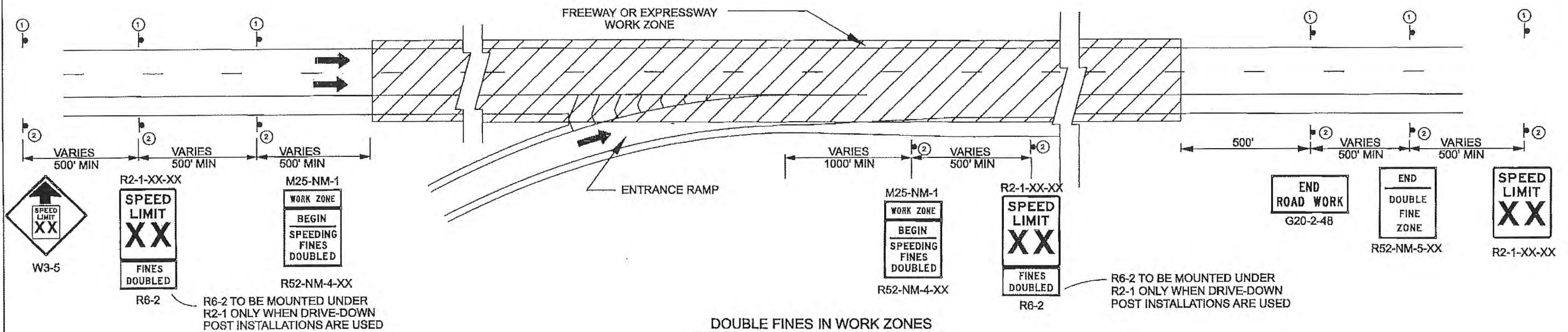
- The contractor shall place reflectorized painted markings on each lane line on each intermediate lift of surfacing or milled surface at the end of the daily surfacing or milling operation. These markings shall be placed in accordance with Figure 1 or Figure 1 A on this sheet, or as directed by the Project Manager.
- In the event the painted markings cannot be placed as described above, the contractor shall, with the approval of the project manager, place marking tape or temporary reflective raised pavement markers. The contractor will be responsible for maintaining the temporary raised pavement markers when requested by the project manager, District Traffic Engineer or their designees. maintenance of the tabs will be considered incidental to the completion of the project.
- The contractor shall place removable marking tape or temporary reflective raised pavement markers after placement of the final lift of surfacing if permanent markings are not placed during the same working day. These markings shall be placed in accordance with Figure 1 or Figure 1 A on this sheet, or as directed by the project manager.
- On roadways with severe curvature, broken-line interim markings with half-cycle lengths and a minimum of two foot (2') stripes or a group of two temporary reflective pavement markings spaced 2 feet apart may be used where passing is allowed. Interim edge lines or channelization lines for delineation may be used as needed or as directed by the project manager. Passing/ no passing zone signing to supplement interim markings for delineation may be used as needed or as directed by the project manager. All interim markings shall be placed in accordance with the current edition of the MUTCD.
- Shoulder and gore area delineation will be required on each intermediate lift of surfacing or milled surface at the end of the day's pavement operation. Payment for marking tape or temporary pavement markings will be paid for under the unit priced of reflectorized painted markings, unless otherwise specified within the contract or Traffic Control Notes. Contractor may substitute edge line pavement marking with traffic control devices such as drums or vertical panels for a maximum 13 day period.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC MARKINGS FOR CONSTRUCTION			
APPROVED	DESIGN ENGINEER		DATE
DESIGNED BY	DRAWN BY	CHECKED BY	
702-01-5/5			



DOUBLE FINES IN WORK ZONES  
SIGNING LAYOUT - STEEL SIGN POST/BASE POST INSTALLATIONS  
AND PORTABLE SIGN SUPPORT INSTALLATIONS

FOR SIGN FACE DETAILS SEE 702-02-2/2



DOUBLE FINES IN WORK ZONES  
SIGNING LAYOUT - ENTRANCE RAMP  
WITHIN LIMITS OF WORK ZONE

NOTES

1. DOUBLE FINE SIGNS (R52-NM-4, R52-NM-5, R2-1) PLACED ON STEEL SIGN POST/BASE POST SIGN SUPPORTS SHALL INCLUDE DEPARTMENT APPROVED BREAKAWAY SYSTEMS AND SHALL BE REMOVED PROMPTLY ONCE WORK ACTIVITIES ARE COMPLETED.
2. DOUBLE FINE SIGNS (R52-NM-4, R52-NM-5, R2-1) INSTALLED ON PORTABLE SIGN SUPPORTS SHALL ONLY UTILIZE SUPPORTS APPROVED BY THE DEPARTMENT. SIGNS SHALL BE PLACED NO MORE THAN TWO HOURS BEFORE WORK ACTIVITIES ARE TO BEGIN AT THE START OF EACH WORK DAY AND SHALL BE REMOVED AT THE END OF EACH WORK DAY WHEN WORKERS ARE NO LONGER PRESENT.
3. DOUBLE FINE SIGNS (R52-NM-4, R52-NM-5, R6-2) SHALL NOT BE USED WHEN WORK ACTIVITIES ARE OUTSIDE THE CLEAR ZONE, FOR SHORT DURATION OPERATIONS (WORK OCCUPYING A LOCATION FOR UP TO 1 HOUR) AND MOBILE OPERATIONS (WORK MOVING INTERMITTENTLY OR CONTINUOUSLY). SEE THE MUTCD (CURRENT EDITION) FOR MORE INFORMATION.

4. DRAWING IS INTENDED TO SHOW SEQUENCE OF DOUBLE FINE SIGNING ONLY AND IS NOT INTENDED TO BE A COMPLETE CONSTRUCTION SIGNING PLAN. SIGNS SHOWN MAY BE COMBINED WITH OTHER WORK ZONE SIGNING THAT MAY INCLUDE BUT IS NOT LIMITED TO ADVANCE WARNING SIGNS, BOPE/EOP SIGNING, SPEED REDUCTION SIGNS, LANE CLOSURES, ETC. SPACING SHALL CONFORM TO THE RECOMMENDATIONS OF THE MOST CURRENT EDITION OF THE MUTCD.
5. DOUBLE FINE SIGNING SEQUENCE SHALL BE REPEATED AT FIVE MILE INTERVALS AND/OR AFTER LOCATIONS WHERE A MAJOR INTERSECTION OR ENTRANCE RAMP OCCURS WITHIN THE LIMITS OF A WORK ZONE.

- ① DOUBLE POSTING OF SIGNS IS REQUIRED ON FACILITIES WITH THREE OR MORE LANES IN EACH DIRECTION AND/OR TRUCK TRAFFIC OF 10% OR MORE.
- ② SPEEDS, DISTANCES AND SIGN SIZES MAY VARY IN ACCORDANCE WITH GUIDELINES IN PART 6 OF THE MUTCD OR AS SHOWN ON TRAFFIC CONTROL PLANS.

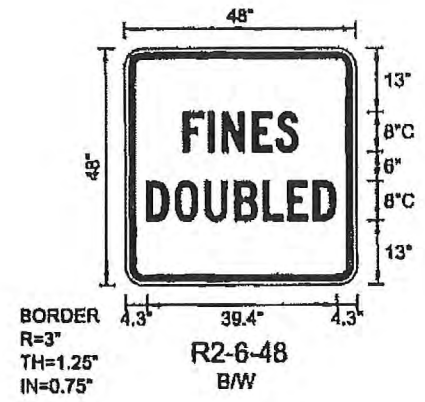
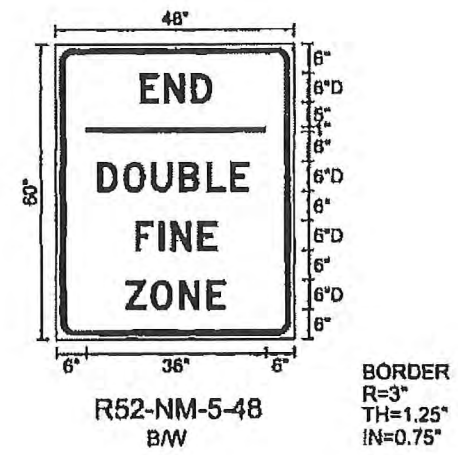
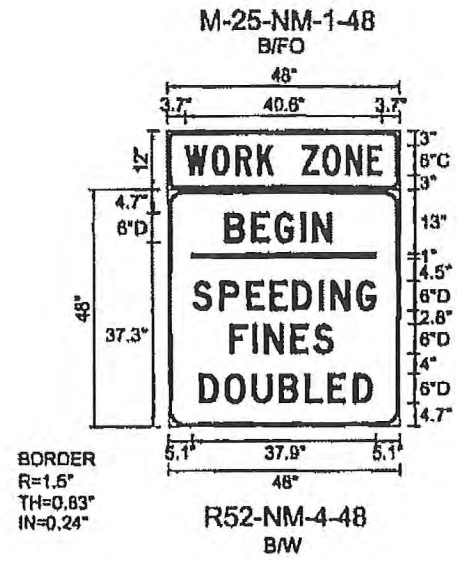


NO.	DATE	REV. BY	DESCRIPTION
1	4/16/08	BLJ	REVISED SIGN & NOTES, ADDED RAMP DETAIL
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
DOUBLE FINES IN WORK ZONES SIGNING LAYOUT			
APPROVED <i>Kathleen Garcia</i> DESIGN ENGINEER			4/16/08 DATE
DESIGNED BY _____		DRAWN BY _____	
		CHECKED BY _____	
702-02-1/2			

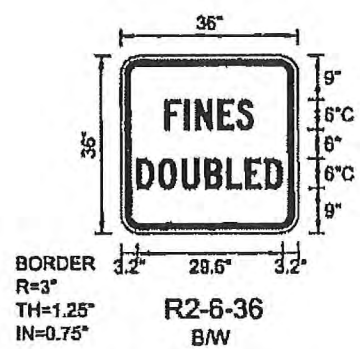
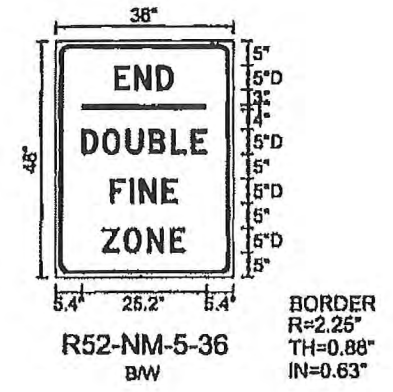
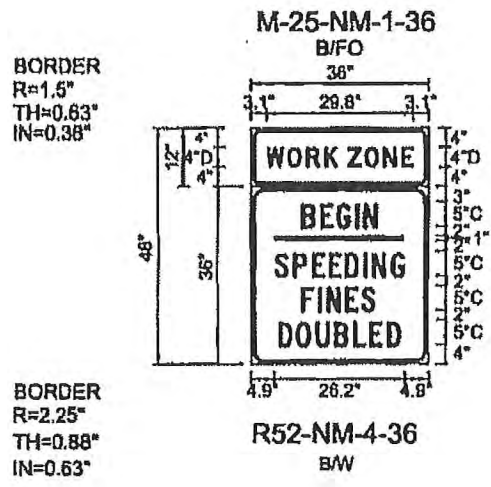


INTERSTATE

NON-INTERSTATE



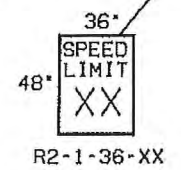
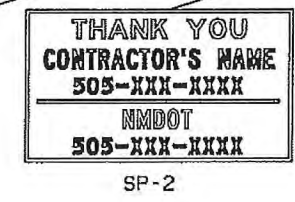
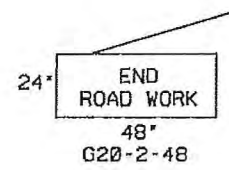
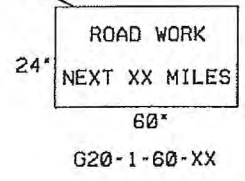
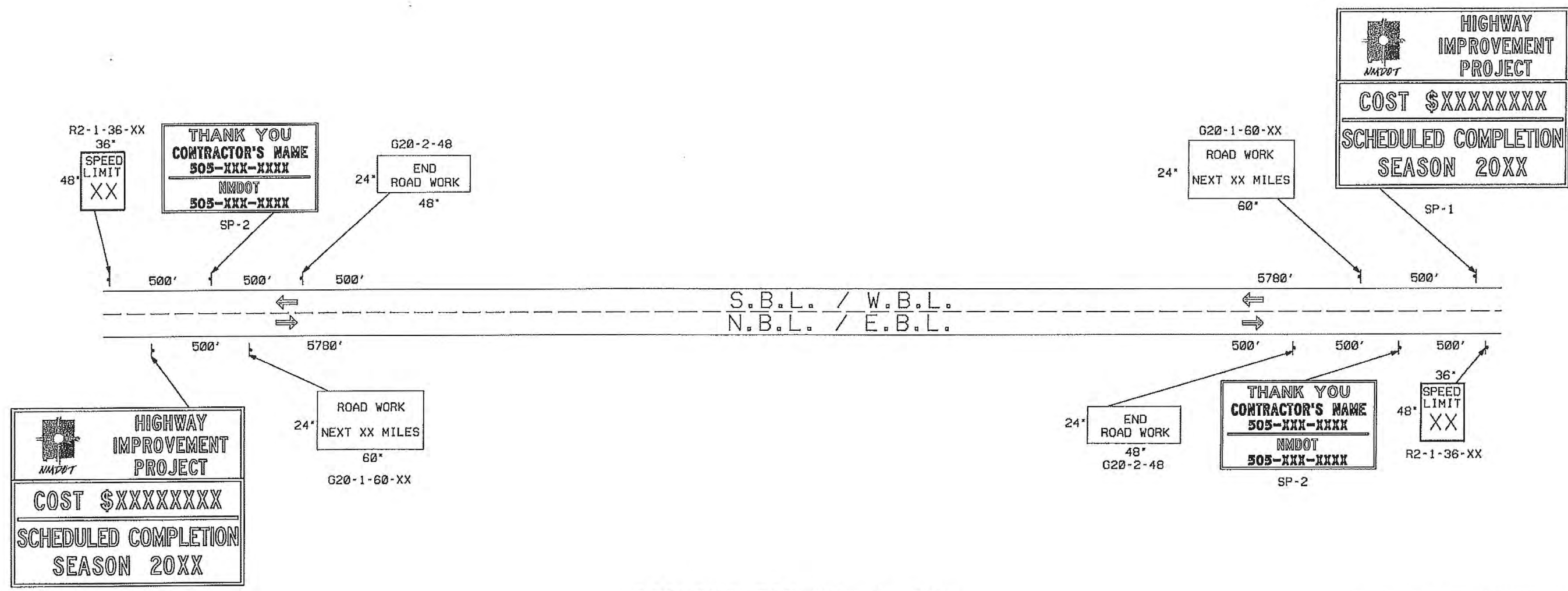
TO BE PLACED BELOW R2-1-48 (SPEED LIMIT SIGN)  
ONLY WHERE DRIVE DOWN POST INSTALLATIONS  
ARE USED.



TO BE PLACED BELOW R2-1-36 (SPEED LIMIT SIGN)  
ONLY WHERE DRIVE DOWN POST INSTALLATIONS  
ARE USED.



NO.	DATE	REV. BY	DESCRIPTION
1	4/18/08	BLJ	REVISED SIGN FACE
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
DOUBLE FINES IN WORK ZONES SIGN FACE DETAILS			
APPROVED	<i>Kathleen Garcia</i>		4/17/08
	DESIGNED BY	DRAWN BY	CHECKED BY
702-02-2/2			



SIGNING FOR BOP & EOP

- NOTES: 1. SIGNS G20-1-60, G20-2-48, R2-1-36-XX, SP-1, AND SP-2 SHALL BE PLACED IN ADVANCE OF THE PROJECT LIMITS AND SHALL REMAIN IN PLACE THROUGH THE DURATION OF THE PROJECT OR AS DIRECTED BY THE PROJECT MANAGER. THESE SIGNS SHALL BE MOUNTED ON A STEEL POST AND BASE POST BREAKAWAY SYSTEM, SEE SERIAL 701-02-1/3, 701-02-2/3, & 701-02-3/3
2. SEE SERIAL 702-01-1/3, FOR SIGN FACE DETAILS.

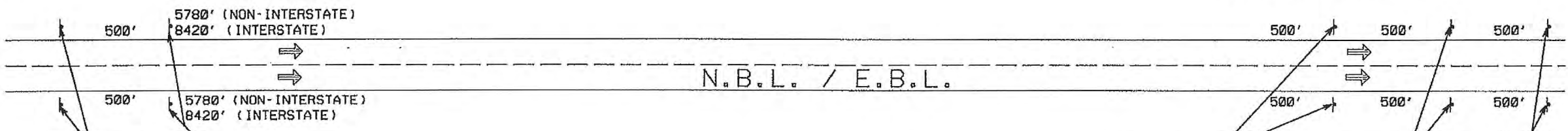
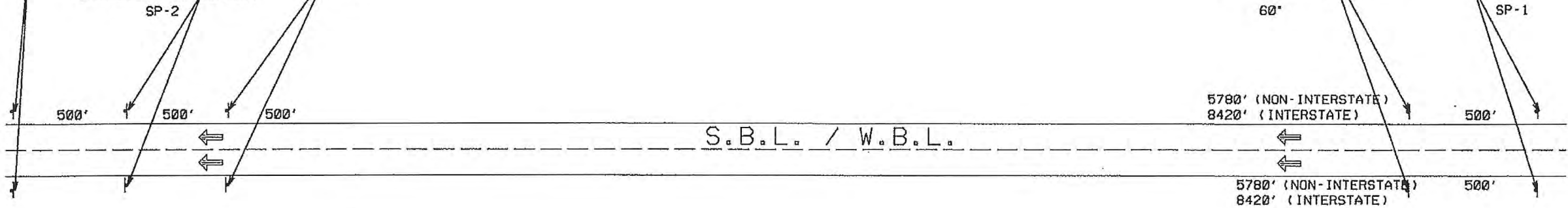
NO.	DATE	REV. BY	DESCRIPTION
REVISIONS ( OR CHANGE NOTICES )			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
BOP/EOP SIGNING (2 LANE)			
APPROVED			7-15-05 DATE
DESIGNED BY	DRAWN BY	CHECKED BY	
702-03-1/1			

R2-1-48-XX (INTERSTATE)  
 R2-1-36-XX (NON-INTERSTATE)  
 36" (48")

48"  
 (60")  
 SPEED  
 LIMIT  
 60

THANK YOU  
 CONTRACTOR'S NAME  
 505-XXX-XXXX  
 HIGHWAY DEPARTMENT  
 505-XXX-XXXX

G20-2-48  
 END  
 ROAD WORK  
 48"



G20-1-60-XX  
 ROAD WORK  
 60"  
 NEXT XX MILES

HIGHWAY  
 IMPROVEMENT  
 PROJECT  
 COST \$XXXXXXXX  
 SCHEDULED COMPLETION  
 SEASON 20XX

HIGHWAY  
 IMPROVEMENT  
 PROJECT  
 COST \$XXXXXXXX  
 SCHEDULED COMPLETION  
 SEASON 20XX

ROAD WORK  
 NEXT XX MILES  
 60"  
 G20-1-60-XX

END  
 ROAD WORK  
 48"  
 G20-2-48

THANK YOU  
 CONTRACTOR'S NAME  
 505-XXX-XXXX  
 HIGHWAY DEPARTMENT  
 505-XXX-XXXX

36" (48")  
 48"  
 (60")  
 SPEED  
 LIMIT  
 XX

R2-1-48-XX (INTERSTATE)  
 R2-1-36-XX (NON-INTERSTATE)

SP-1

SP-2

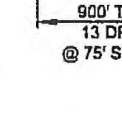
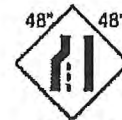
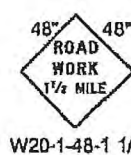
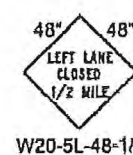
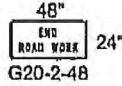
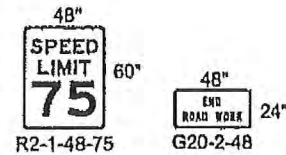
SIGNING FOR BOP & EOP

- NOTES: 1. SIGNS G20-1-60, G20-2-48, R2-1-36-XX, SP-1, AND SP-2 SHALL BE PLACED IN ADVANCE OF THE PROJECT LIMITS AND SHALL REMAIN IN PLACE THROUGH THE DURATION OF THE PROJECT OR AS DIRECTED BY THE PROJECT MANAGER. THESE SIGNS SHALL BE MOUNTED ON A STEEL POST AND BASE POST BREAKAWAY SYSTEM, SEE SERIAL 701-02 (3 SHEETS).
2. SEE SERIAL 702-02-1/1, FOR SIGN FACE DETAILS.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS ( OR CHANGE NOTICES )			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
BOP/EOP SIGNING (4 LANE)			
APPROVED			DATE
DESIGNED BY		DRAWN BY	CHECKED BY
702-04-1/1			



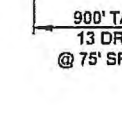
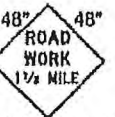
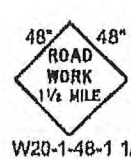
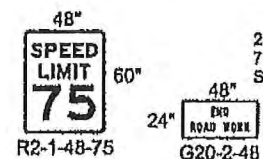
NOTE:  
W3-5a-48-65 CAN BE USED  
INSTEAD OF W3-5-48-65.



\* LONGITUDINAL BUFFER SPACE

SPEED (MPH)	LENGTH (FT)	DRUMS (EA)	SPACING (FT)
75	820'	11	150'

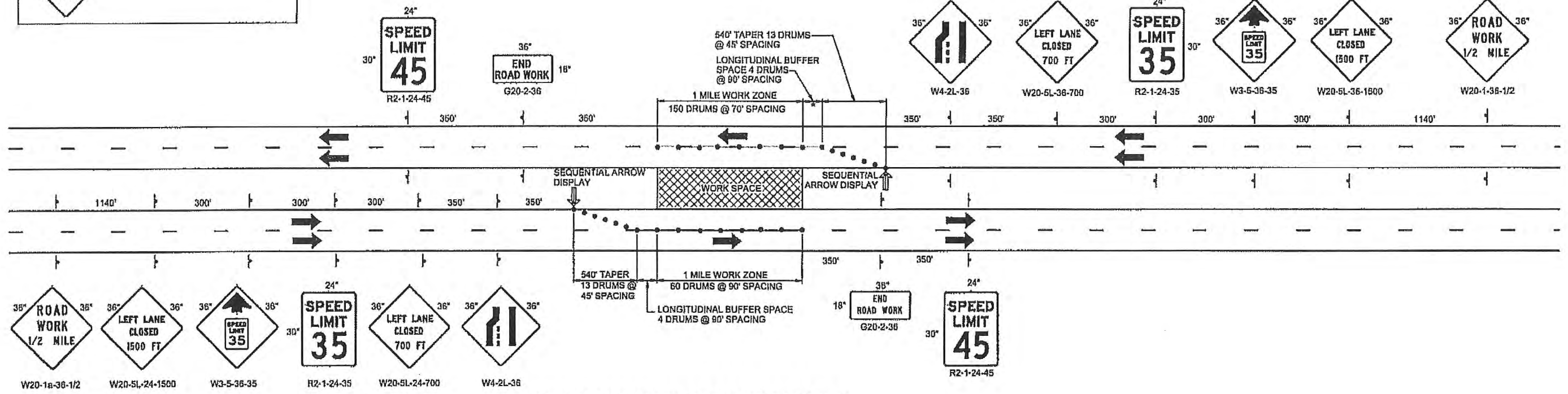
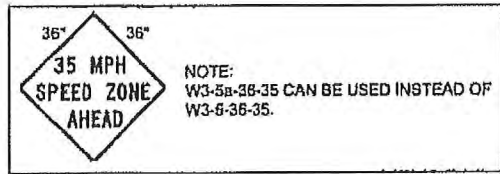
**INSIDE LANES & MEDIAN OPERATIONS**



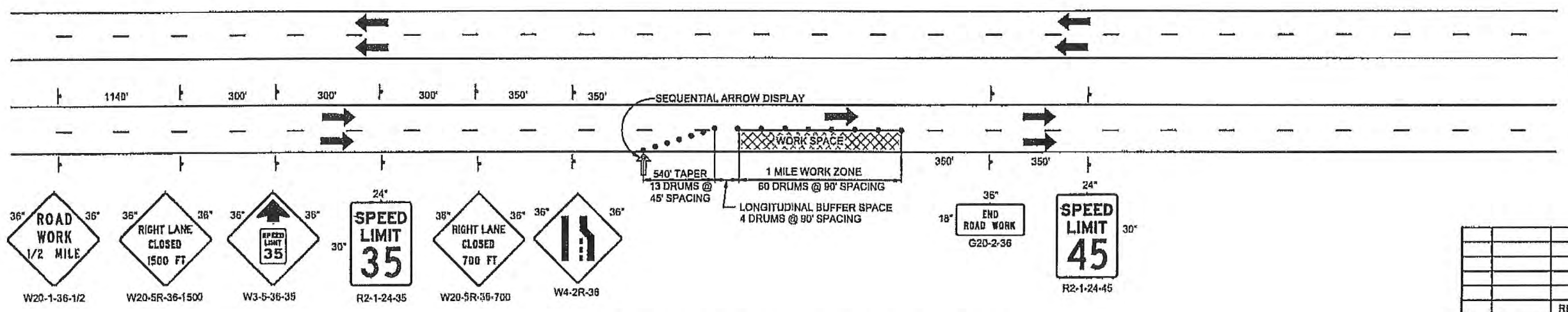
**OUTSIDE LANE OPERATIONS**

- NOTES:
- 1.) JUSTIFICATION FOR SPEED REDUCTION IN EXCESS OF 10 MPH IS REQUIRED. TRAINED OR CERTIFIED PERSONNEL SHOULD USE ENGINEERING JUDGEMENT TO DETERMINE IF SPEED REDUCTION IS WARRANTED.
  - 2.) PORTABLE CHANGEABLE MESSAGE SIGN SHOULD BE USED TO WARN OF SPEED REDUCTION, NARROW LANES OR PRESENCE OF WORKERS NEAR TRAFFIC.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
INSIDE LANE / MEDIAN AND OUTSIDE LANE OPERATIONS (INTERSTATE, 75 MPH)			
APPROVED:		11/6/15	
DESIGNED BY:		DRAWN BY:	
		CHECKED BY:	
702-09-1/2			



MEDIAN CONSTRUCTION OPERATION

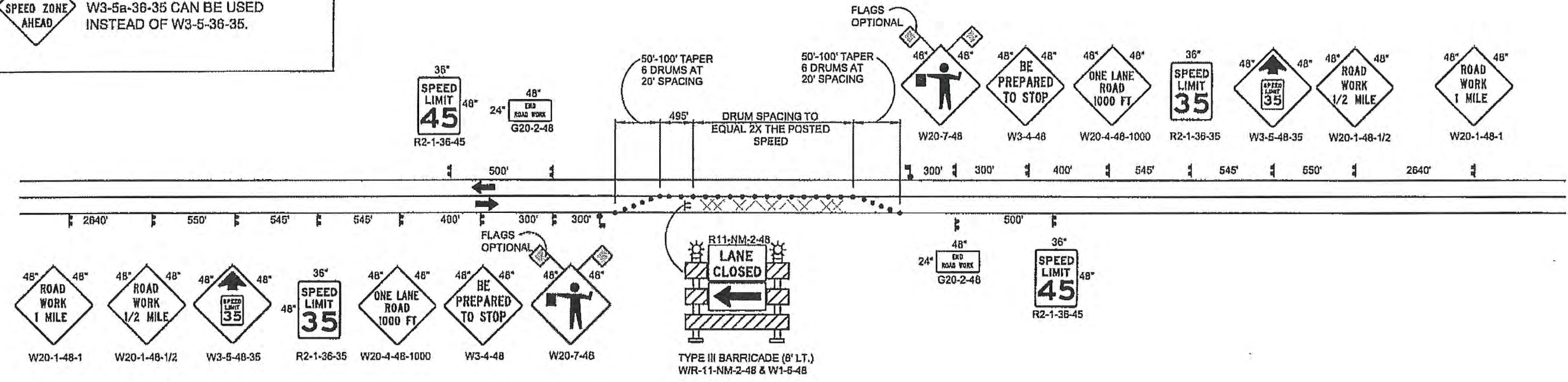


OUTSIDE LANE & SHOULDER OPERATION

- NOTES:
- PORTABLE CHANGABLE MESSAGE SIGNS SHOULD BE USED TO WARN OF SPEED REDUCTION, NARROW LANES OR PRESENCE OF WORKERS.
  - ADDITIONAL SPEED REDUCTION MAY BE CONSIDERED BASED ON FIELD CONDITIONS INCLUDING BUT NOT LIMITED TO:
    - LATERAL OFFSET TO WORKERS AND EQUIPMENT.
    - LACK OF SHY DISTANCE FOR MOTORISTS
    - NARROW LANES, ETC.

REV. BY	DESCRIPTION
REVISIONS ( OR CHANGE NOTICES )	
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING	
MEDIAN CONSTRUCTION AND OUTSIDE LANE AND SHOULDER OPERATIONS (45 MPH)	
APPROVED: <i>A. Shin</i>	DATE: 11/6/15
DESIGNED BY: _____	CHECKED BY: _____
702-10-1/3	

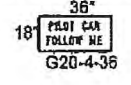
36" 36"  
35 MPH  
SPEED ZONE  
AHEAD  
NOTE:  
W3-5a-36-35 CAN BE USED  
INSTEAD OF W3-5-36-35.



TYPICAL SIGNING AND DRUM PLACEMENT  
WORKING HOURS - FOR DETOUR CONNECTIONS AND OBLITERATION

- LEGEND:**
- TYPE B WARNING LIGHT
  - FLAGGER STATION
  - FLUORESCENT ORANGE FLAG

- NOTES:**
- DURING CONSTRUCTION OPERATION (ONE LANE TRAFFIC), TRAFFIC SHALL BE MOVED THROUGH THE WORK ZONE USING PILOT CARS. ALL PILOT CARS SHALL BE EQUIPPED WITH THE FOLLOWING SIGNS AND RADIO FOR FLAGMAN & PILOT CARS.



- THE PILOT CAR, APPLICABLE SIGN, AND OTHER ITEMS RELATED TO THE USE OF THE PILOT CAR SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION. NO PAYMENT OR MEASUREMENT WILL BE MADE THEREFORE.
- AT THE END OF THE WORKING DAY, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE A DRIVING SURFACE FREE OF OBSTRUCTION (TWO WAY TRAFFIC).
  - BUFFERS SHOULD BE ADJUSTED TO INCREASE VISIBILITY OF FLAGGER STATIONS.
  - TO ENHANCE VISIBILITY OF WORK ZONE USE 48"x48" WARNING SIGNS.

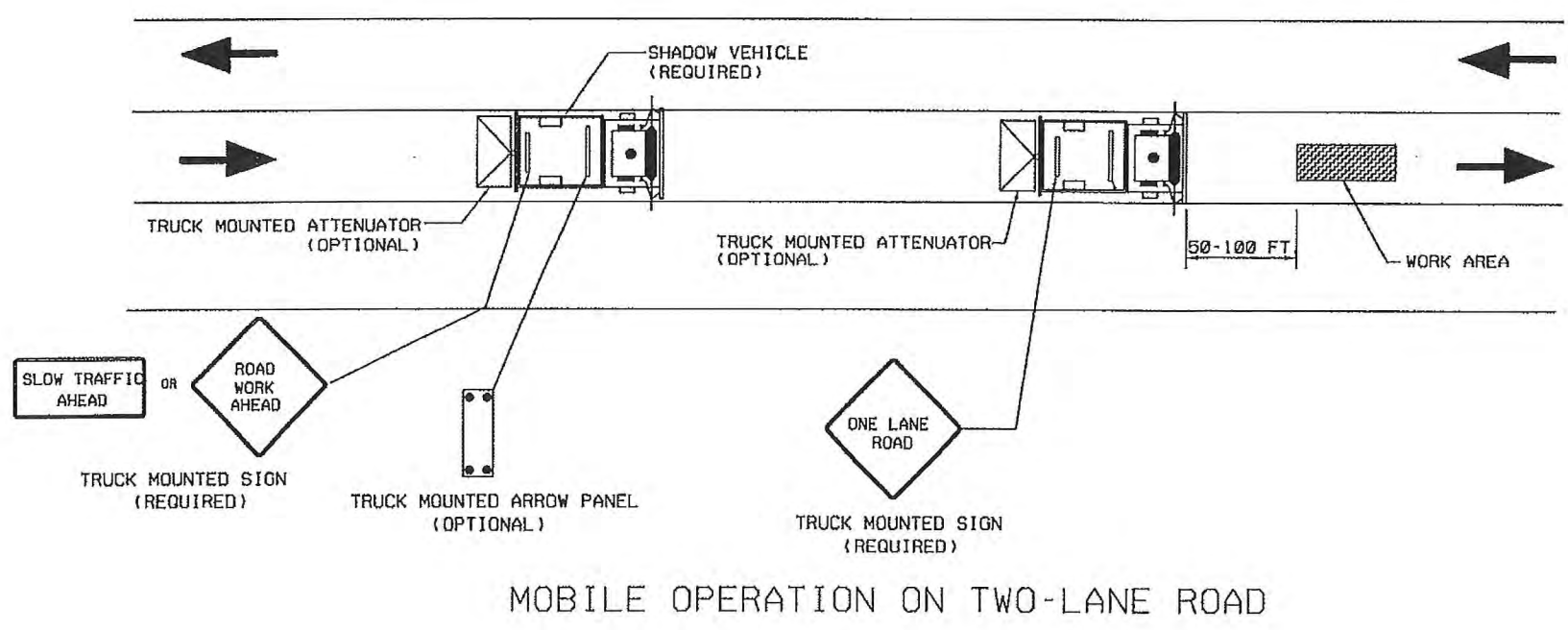
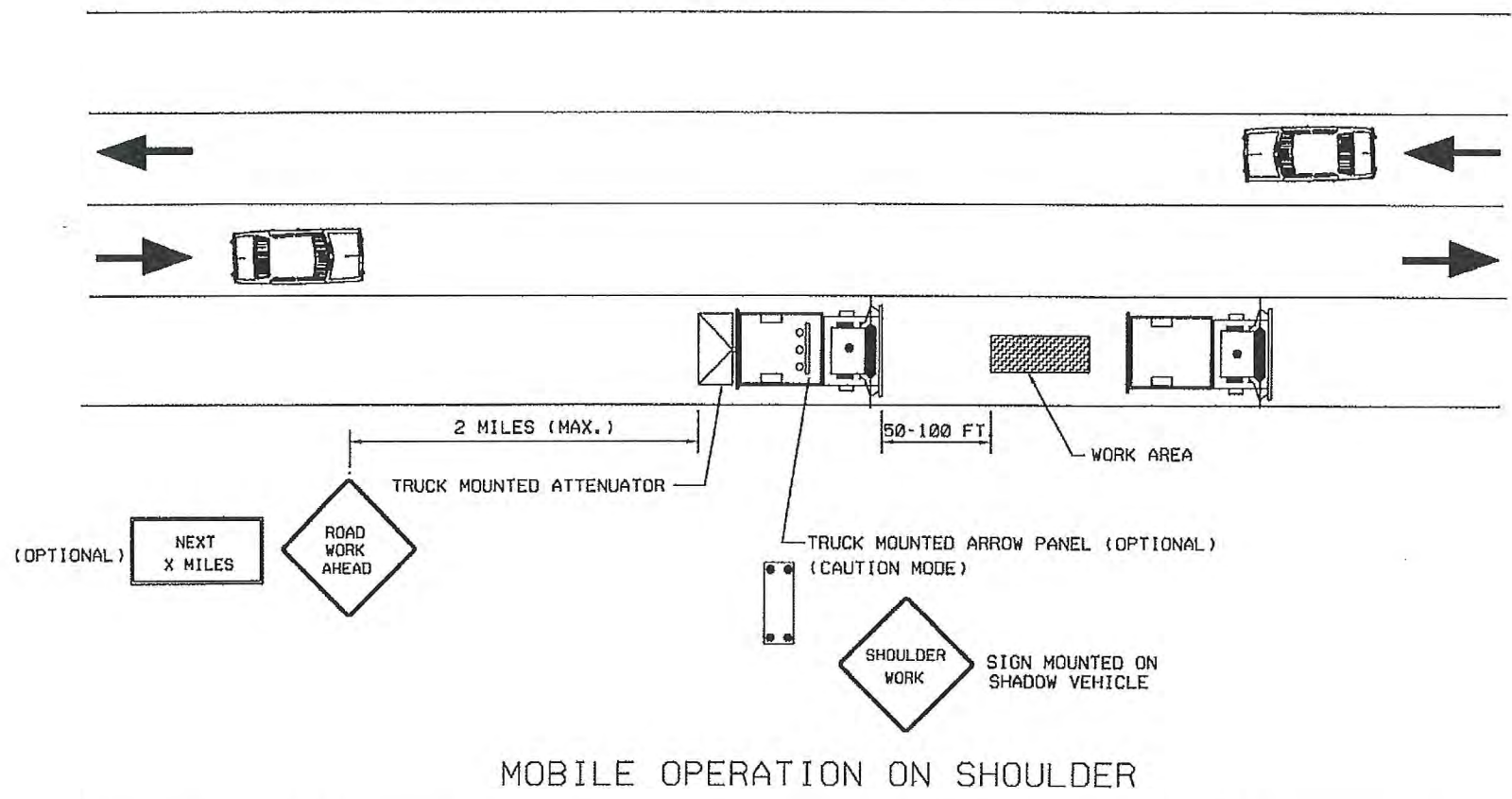
LONGITUDINAL BUFFER

SPEED (MPH)	LENGTH (FEET)	DRUMS (EACH)	SPACING (FEET)
45	360 MIN.	8	45 (MAX)

\* DEVICE SPACING MAY BE REDUCED AND ADDITIONAL DEVICES USED AS NEEDED.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TWO LANE ROADWAY WITH ONE-LANE CLOSURE, FLAGMAN, PILOT CAR (45 MPH)			
APPROVED	[Signature]		DATE
	DESIGN ENGINEER		11/6/15
702-11-2/2			

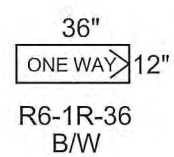
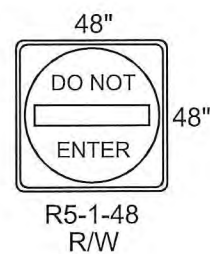
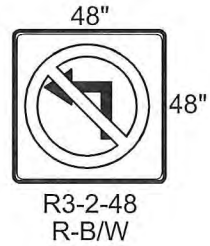
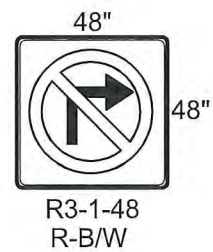
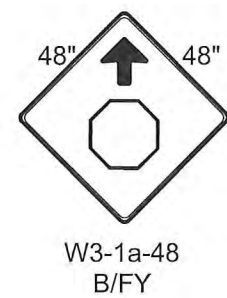
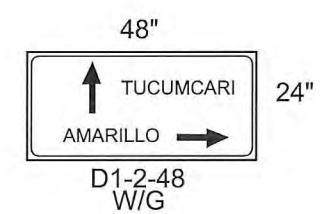
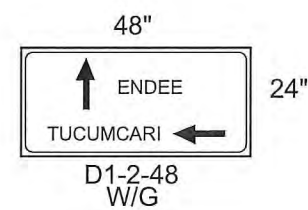
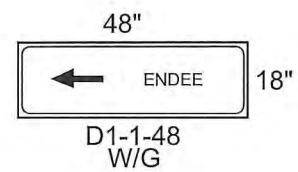
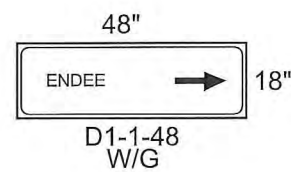
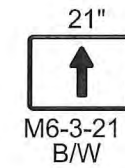
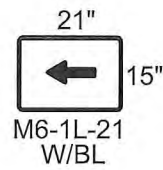
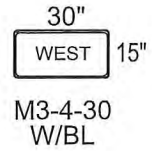
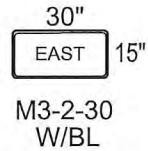
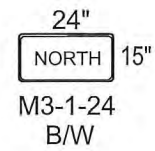
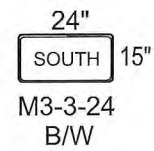
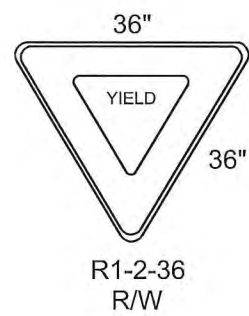
DRAWING SCALE: NOT TO SCALE



**NOTES:**

- 1.) VEHICLE MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN AT A MINIMUM OF 4 FT. ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- 2.) ADDITIONAL SHADOW VEHICLES ON TWO LANE ROAD TO WARN AND REDUCE THE SPEED OF ON COMING OR OPPOSING MOTOR VEHICLE TRAFFIC MAY BE USED.
- 3.) SEE MUTCD, CURRENT EDITION FOR ADDITIONAL GUIDANCE NOTES.

NO.	DATE	REV. BY	DESCRIPTION
REVISIONS ( OR CHANGE NOTICES )			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
MAINTENANCE TRAFFIC CONTROL PLANS, MOBILE OPERATION ON SHLDR. & TWO LANE			
APPROVED		DESIGN ENGINEER	DATE
			2-23-08
DESIGNED BY		DRAWN BY	CHECKED BY
702-12-1/2		SHEET 1 OF 2	



4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT  
OF TRANSPORTATION

PERMANENT SIGNING



I-40, EB				SIGN CODE	NO. OF SIGNS	TOTAL SIGN AREA SQ.FT.	POST LENGTHS				MOUNTING REQUIREMENTS**					BASE POST: 2-1/2 x 2-1/2	
STATION	LOCATION	LENGTH OF SIGN INCHES	WIDTH OF SIGN INCHES				LEFT	CTR.	RIGHT	TOTAL	DRIVEDOWN POSTS					NO.	TOTAL LENGTH
											1-3/4 X 1-3/4	2 X 2	2-1/4 X 2-1/4	2-3/16 X 2-3/16	2-1/2 X 2-1/2		
<b>EAST BOUND LANE</b>																	
-	EAST OFF RAMP	48	48	R5-1-48	2	32.00	21		21	42.00						4	14.0
-	EAST OFF RAMP	30	48	R5-1a-48	4	40.00	42		42	84.00						8	28.0
-	EAST OFF RAMP	48	48	W3-1-48	2	32.00	23		23	46.00						4	14.0
-	EAST OFF RAMP	30	48	R5-1a-48	2	20.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	48	48	R3-1-48	1	16.00		12		12.00						1	3.5
-	EAST OFF RAMP	48	48	R5-1-48	2	32.00		24		24.00						2	7.0
-	EAST OFF RAMP	48	48	R1-1-48	2	32.00	23		23	46.00						4	14.0
-	EAST OFF RAMP	12	36	R6-1R-36	1	3.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	12	36	R6-1L-36	1	3.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	48	48	R3-2-48	1	16.00		12		12.00						1	3.5
-	EAST OFF RAMP	18	48	D1-1-48	1	6.00		12		12.00						1	3.5
-	EAST OFF RAMP	15	24	M3-1-24	1	2.50		12		12.00						1	3.5
-	EAST OFF RAMP	30	30	M1-NM-5-30	1	6.25							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	15	21	M6-1L-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	15	24	M3-3-24	1	2.50		12		12.00						1	3.5
-	EAST OFF RAMP	30	30	M1-NM-5-30	1	6.25							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	15	21	M6-1R-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST OFF RAMP	36	36	R1-2-36	1	9.00		12		12.00						1	3.5
-	EAST INTERCHANGE	15	24	M3-3-24	1	2.50		12		12.00						1	3.5
-	EAST INTERCHANGE	30	30	M1-NM-5-30	1	6.25							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	21	M6-3-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	30	M3-2-30	1	3.13		12		12.00						1	3.5
-	EAST INTERCHANGE	36	36	M1-1-36-40	1	9.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	21	M6-1L-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	36	12	OM-3L-12	1	3.00		12		12.00						1	3.5
-	EAST INTERCHANGE	36	12	OM-3R-12	1	3.00		12		12.00						1	3.5
-	EAST INTERCHANGE	24	48	D1-2-48	1	8.00		12		12.00						1	3.5
-	EAST INTERCHANGE	15	30	M3-4-30	1	3.13		12		12.00						1	3.5
-	EAST INTERCHANGE	36	36	M1-1-36-40	1	9.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	21	M6-3-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	30	M3-2-30	1	3.13		12		12.00						1	3.5
-	EAST INTERCHANGE	36	36	M1-1-36-40	1	9.00							SIGN MOUNTED W/ OTHER SIGN				
-	EAST INTERCHANGE	15	21	M6-1R-21	1	2.19							SIGN MOUNTED W/ OTHER SIGN				
-	EAST ON RAMP	30	24	R8-3-24	1	5.00		12		12.00						1	3.5
<b>TOTAL</b>						<b>335.75</b>				<b>410.00</b>							<b>126.00</b>
<b>USE</b>						<b>336.00</b>				<b>410.00</b>							<b>126.00</b>

\*\* SEE NEW MEXICO STANDARD, 701-02 & 701-03, FOR MOUNTING REQUIREMENTS.

\*\* CONTRACTOR WILL BE RESPONSIBLE TO LOG AND INVENTORY ALL EXISTING PERMANENT SIGNING PRIOR TO REMOVAL AND MUST BE INSTALLED BACK IN ORIGINAL LOCATIONS.

EAST BOUND LANE				
ITEM NO.	BID ITEM	UNITS	TOTAL	USE
701000	PANEL SIGNS	SQ.FT.	336.00	336.00
701100	STEEL POST AND BASE POST FOR ALUMINUM PANEL SIGNS	LIN.FT.	536.00	536.00



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NO.	DESCRIPTION	DATE	BY
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NEW MEXICO DEPARTMENT OF TRANSPORTATION			
PERMANENT SIGNING QUANTITIES INTERCHANGE SIGNING			



ITEM NO. 704002, RETROREFLECTORIZED PAINTED MARKINGS 6"

STATION TO STATION	LENGTH FT.	LOCATION	DESCRIPTION
<b>RAMPS</b>			
WB OFF RAMP	1,169.00	RT.	6" SOLID WHITE STRIPE
WB OFF RAMP	715.00	LT.	6" SOLID YELLOW STRIPE
WB ON RAMP	1,825.00	RT.	6" SOLID WHITE STRIPE
WB ON RAMP	1,260.00	LT.	6" SOLID YELLOW STRIPE
EB OFF RAMP	1,406.00	RT.	6" SOLID WHITE STRIPE
EB OFF RAMP	1,345.00	LT.	6" SOLID YELLOW STRIPE
EB ON RAMP	1,608.00	RT.	6" SOLID WHITE STRIPE
EB ON RAMP	1,575.00	LT.	6" SOLID YELLOW STRIPE
<b>ISLAND</b>			
WB OFF RAMP	100.00	-	6" SOLID WHITE STRIPE
EB OFF RAMP	100.00	-	6" SOLID WHITE STRIPE
<b>RAMPS</b>			
WB OFF RAMP	64.00	EXT 369 STOP BAR*	6" SOLID WHITE STRIPE
EB OFF RAMP	64.00	EXT 369 STOP BAR*	6" SOLID WHITE STRIPE
<b>TOTAL USE</b>	<b>11,231.00</b>		
<b>EXT 369 INTERCHANGE</b>			
EXT 369 INTERCHANGE	1,000.00	LT.	6" SOLID WHITE STRIPE
EXT 369 INTERCHANGE	1,000.00	RT.	6" SOLID WHITE STRIPE
EXT 369 INTERCHANGE	1,000.00	C.L.	6" SOLID YELLOW STRIPE
EXT 369 INTERCHANGE	1,000.00	C.L.	6" SOLID YELLOW STRIPE
<b>TOTAL USE</b>	<b>4,000.00</b>		
<b>PROJECT TOTAL @ 2 APPLICATIONS</b>	<b>15,231.00</b>		
<b>PROJECT USE</b>	<b>30,462.00</b>		

NOTE: RETROREFLECTORIZED PAINTED MARKINGS SHALL BE 2 HI-BUILD PAINT APPLICATIONS.  
 NOTE: CONTRACTOR SHALL MATCH EXISTING STRIPING LAYOUT  
 \* STOP BAR WILL CONSIST OF 4-6" X 16' SOLID WHITE STRIPES TO MAKE A 24" X 16' STOP BAR.

ITEM NO. 704000, RETROREFLECTORIZED PAINTED MARKINGS 4"

STATION TO STATION	LENGTH FT.	LOCATION	DESCRIPTION
<b>FR-4132</b>			
EB	47,520.00	RT.	4" SOLID WHITE STRIPE
WB	47,520.00	RT.	4" SOLID WHITE STRIPE
CENTER	11,880.00	C.L.	4" DASHED YELLOW CENTER STRIPE
CENTER	16,791.00	C.L.	4" SOLID YELLOW STRIPE
<b>TOTAL USE</b>	<b>123,711.00</b>		
<b>PROJECT TOTAL @ 2 APPLICATIONS</b>	<b>247,422.00</b>		
<b>PROJECT USE</b>	<b>247,422.00</b>		

NOTE: RETROREFLECTORIZED PAINTED MARKINGS SHALL BE 2 HI-BUILD PAINT APPLICATIONS.  
 NOTE: CONTRACTOR SHALL MATCH EXISTING STRIPING LAYOUT

ITEM NO. 704011, RETROREFLECTORIZED PAINTED MARKING THRU ARROW

STATION/LOCATION	EACH	DESCRIPTION
EXIT 369	2.00	REFERENCE SHEET 701-18-1/2
<b>PROJECT TOTAL</b>	<b>2.00</b>	
<b>PROJECT USE</b>	<b>2.00</b>	

ITEM NO. 704030, RETROREFLECTORIZED PAINTED MARKING WRONG WAY ARROW

STATION/LOCATION	EACH	DESCRIPTION
EXIT 369	4.00	REFERENCE SHEET 701-18-1/2
<b>PROJECT TOTAL</b>	<b>4.00</b>	
<b>PROJECT USE</b>	<b>4.00</b>	

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NEW MEXICO DEPARTMENT OF TRANSPORTATION

PERMANENT SIGNING QUANTITIES



**PERMANENT SIGNING & STRIPING NOTES**

1. ALL REGULATORY, WARNING, SPECIAL, AND GUIDE SIGNS SHALL COMPLY WITH THE FOLLOWING REQUIREMENTS:

SIGN	SIGN CODE	COLOR	LETTER SHEETING	BACKGROUND SHEETING
STOP	R1-1	(W/RED)	TYPE VIII OR IX	TYPE VIII OR IX
YIELD	R1-2	(W/RED)	TYPE VIII OR IX	TYPE VIII OR IX
DO NOT ENTER	R5-1	(W/RED)	TYPE VIII OR IX	TYPE VIII OR IX
WRONG WAY	R5-1a	(W/RED)	TYPE VIII OR IX	TYPE VIII OR IX
SCHOOL BUS SIGN	S3-1	(BLK/FLOURESCENT YELLOW GREEN)	TYPE VIII OR IX	TYPE VIII OR IX
SCHOOL BUS XING	S1-1	(BLK/FLOURESCENT YELLOW GREEN)	TYPE VIII OR IX	TYPE VIII OR IX
PEDESTRIAN XING	W11-2	(BLK/FLOURESCENT YELLOW GREEN)	TYPE VIII OR IX	TYPE VIII OR IX
BIKE XING	W11-1	(BLK/FLOURESCENT YELLOW GREEN)	TYPE VIII OR IX	TYPE VIII OR IX
ALL SCHOOL/PED/BIKE SUPPLEMENTAL PLAQUES		(BLK/FLOURESCENT YELLOW GREEN) WHEREVER BLK/YELLOW IS SPECIFIED AS COLOR IN MUTCD	TYPE VIII OR IX	TYPE VIII OR IX
ALL OTHER WARNING SIGNS & SUPPLEMENTAL PLAQUES		(BLK/FLOURESCENT YELLOW) WHEREVER BLK/YELLOW IS SPECIFIED AS COLOR IN MUTCD	TYPE VIII OR IX	TYPE VIII OR IX
GUIDE & DESTINATION SIGNS INCLUDE EXTRUDED & NON-EXTRUDED (OVERHEAD & GROUND MOUNTED)	ALL	(W/GREEN) EXCEPT WHERE SPECIFIED IN THE MUTCD	TYPE VIII OR IX	TYPE IV
E-CODE SIGNS	E 11-1, 1a, 1b, 1c	(BLK/FLOURESCENT YELLOW)	TYPE VIII OR IX	TYPE IV
RECREATIONAL & CULTURAL INTERESTS	ALL	(W/BROWN)	TYPE VIII OR IX	TYPE IV
GENERAL SERVICE	ALL	(W/BLUE)	TYPE VIII OR IX	TYPE IV
SPECIFIC SERVICE	ALL	(W/BLUE)	TYPE VIII OR IX	TYPE IV
ALL OTHER PERMANENT SIGNS		(BLK/WHITE) EXCEPT WHERE SPECIFIED OTHERWISE IN THE MUTCD	TYPE IV	TYPE IV

**PERMANENT SIGNING & STRIPING NOTES (CON'T)**

- ONLY 0.125 INCHES THICK ALUMINUM PANEL SIGNS ARE PERMITTED FOR WARNING AND REGULATORY SIGNS.
- ANTI GRAFFITI COATING SHALL BE APPLIED ON ALL REGULATORY, WARNING, GUIDE, B
- QUANTITIES MAY VARY AS FIELD CONDITIONS DICTATE. THE CONTRACTOR WILL BE PAID FOR ACTUAL QUANTITIES USED.
- ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH N.M. STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION (LATEST EDITION) AND ANY APPLICABLE SPECIAL PROVISION AND/OR SUPPLEMENTAL SPECIFICATION. ALSO THE DEVICES SHALL COMPLY WITH CURRENT EDITION, WITH REVISIONS, OF THE MUTCD.
- EACH SIGN FACE SHOWN ON PLANS SHALL MEET THE SPECIFICATIONS IN THE STANDARD HIGHWAY SIGNS MANUAL (CURRENT EDITION) FOR PROPER ARRANGEMENT, SPACING OF LETTERS, LETTER HEIGHT, SYMBOLS AND BORDERS FOR THE SPECIFIED SIZE AND MESSAGE AS SHOWN ON PLANS.
- POST LENGTHS ARE BASED ON A MINIMUM OF 5 FT FOR RURAL ROADWAY SECTIONS TO A MINIMUM OF 7 FT FOR URBAN AND INTERSTATE ROADWAY SECTIONS. THE LENGTHS ARE MEASURED FROM THE BOTTOM OF THE SIGN TO NEAR EDGE OF THE DRIVING LANE.
- THE LATERAL CLEARANCE OF SIGNS SHALL BE NO CLOSER THAN THE 6 FT FROM THE EDGE OF SHOULDER OR 12 FT FROM THE EDGE OF TRAVELED WAY FOR RURAL AREAS AND NO CLOSER THAN 2 FT FROM FACE OF CURB FOR URBAN AREAS.
- ALL SIGNING HARDWARE, INCLUDING BRACKETS, (FOR MOUNTING ALL STREET NAME SIGNS, W1-8-18 BACK TO BACK MOUNTING, ETC.) ARE CONSIDERED INCIDENTAL TO SIGN INSTALLATION. THEREFORE, NO PAYMENT WILL BE MADE.
- THREE 4 LB/FT SIGN POST INSTALLATION APPROVED ONLY IF THE SPAN BETWEEN THE OUTER POSTS EXCEEDS 84" (SIGN WIDTH OF 96" OR GREATER).
- 3'- 6" BASE POSTS ARE REQUIRED FOR ALL SMALL SIGN POSTS. SEE STANDARD DRAWING 701-02-1/3 FOR DETAILS.
- SIGN POSTS SHALL BE SQUARE TUBING AS APPROVED BY THE PROJECT MANAGER OR THE DISTRICT TRAFFIC ENGINEER.
- CONTRACTOR SHALL FIELD VERIFY ALL SIGNING INFORMATION, TEXT, AND LOCATIONS PRIOR TO FABRICATION AND INSTALLATION. NEW SIGN LOCATIONS SHALL BE APPROVED BY THE PROJECT MANAGER OR THE DISTRICT TRAFFIC ENGINEER.
- WATER BLASTING IS THE ONLY APPROVED METHOD OF EXISTING STRIPE REMOVAL. THE USE OF BLACK PAINT AS A METHOD OF REMOVING OR COVERING EXISTING STRIPING IS STRICTLY PROHIBITED.
- ALL BASE POSTS WILL BE 2 1/2" X 2 1/2" AND ALL DRIVEDOWN POSTS WILL BE 2 1/4" X 2 1/4".



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