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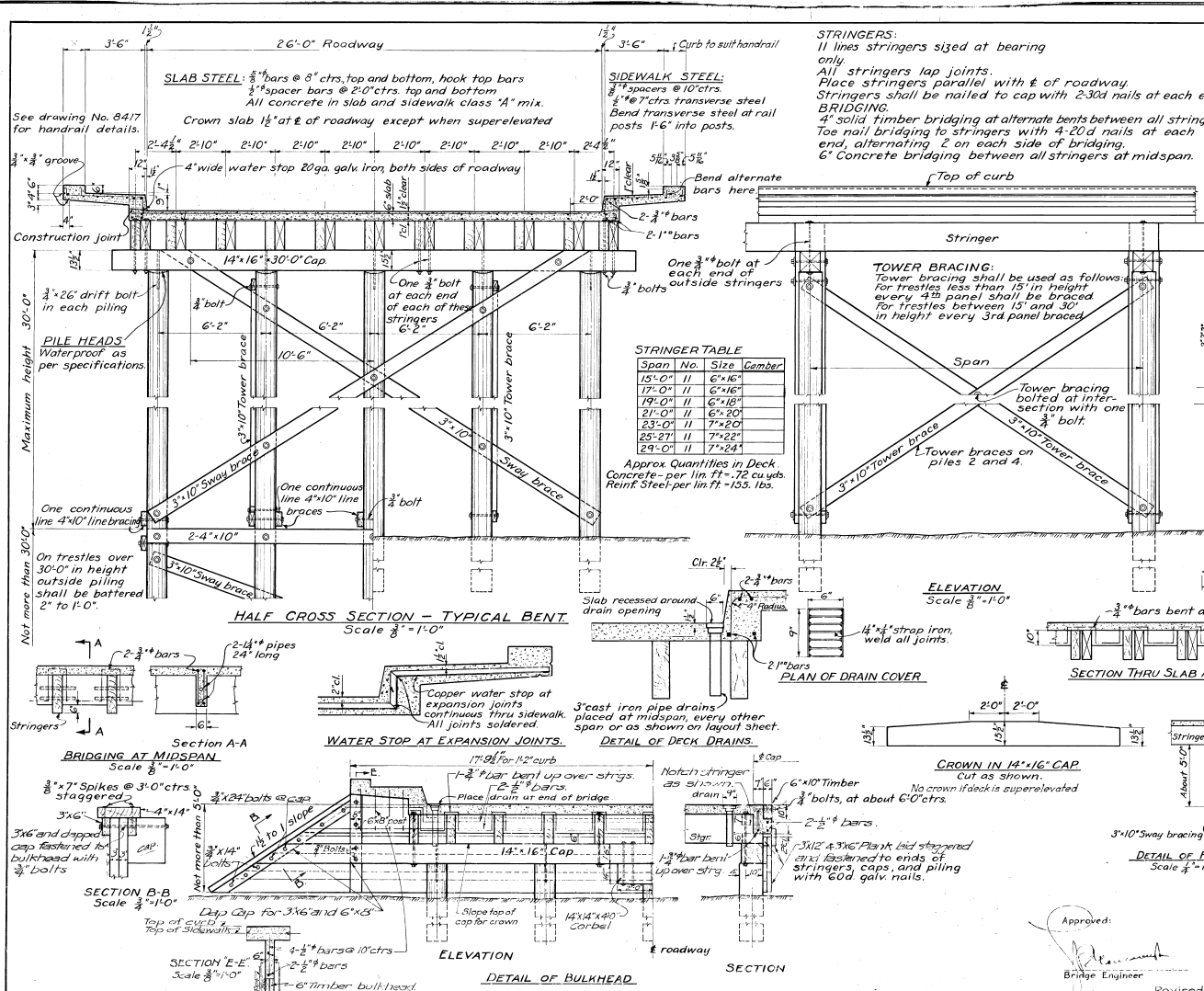
H. H. H. H.

Acting Bridge Engineer

State Highway Engineer

OREGON
STATE HIGHWAY COMMISSION
WEST DAIRY CREEK BRIDGES
ON
DAVIES-BANKS SECTION
NEHALEM SECONDARY HWY.-WASHINGTON CO.
PLANS AND ELEVATIONS

SCALE AS NOTED DRAWN BY J.D.E. SHEET 1 OF 3
JUNE 10, 1938 TRACED BY J.D.E. CHECKED BY
BRIDGES NOS. 2302 & 2303 DRAWING NO. 5947
ACCOMPANIED BY DRWGS. 5451 & 5735



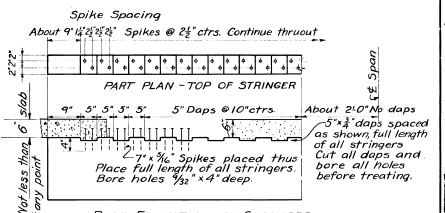
STRINGERS:
 11 lines stringers sized at bearing only.
 All stringers lap joints.
 Place stringers parallel with $\frac{1}{2}$ of roadway.
 Stringers shall be nailed to cap with 2-30d nails at each end.
BRIDGING:
 4" solid timber bridging at alternate bents between all stringers.
 Toe nail bridging to stringers with 4-20d nails at each end, alternating 2 on each side of bridging.
 6" concrete bridging between all stringers at midspan.

GENERAL NOTES:
 All lumber and piling shall be **PRESSURE TREATED Douglas Fir.**
 All lumber shall be rough except as otherwise specified.
 All lumber shall be framed and holes bored before treating with creosote.
 All reinforcing steel shall be deformed bars.
 All bolts shall be $\frac{3}{4}$ inch machine bolts with square heads and nuts, fitted with a cast iron, O.G. or malleable iron washer at each end.
 All hardware including nails shall be galvanized.
 All material and workmanship shall conform to the Specifications for bridges of the Oregon State Highway Commission, except the grading of lumber and timber which shall be in accordance with the Standard Specifications for Structural Timber.
 Trestle designed for H-15 loading.

STRINGER TABLE

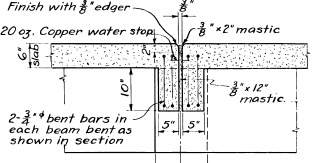
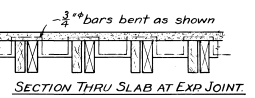
Span No.	Size	Gamber
15'-0"	11	6"x6"
17'-0"	11	6"x16"
19'-0"	11	6"x18"
21'-0"	11	6"x20"
23'-0"	11	7"x20"
25'-0"	11	7"x24"
29'-0"	11	7"x24"

Approx. Quantities in Deck
 Concrete - per lin. ft. = .72 cu. yds.
 Reinf. Steel - per lin. ft. = 153 lbs.



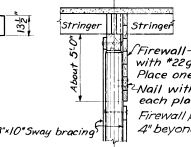
DETAIL OF DAPS AND SHEAR NAILS IN ALL STRINGERS.
 Scale 3/4" = 1'-0"

ELEVATION
 Scale 3/8" = 1'-0"



SECTION THRU EXPANSION JOINT.
 Locate at every other bent or as shown on layout sheet.

CROWN IN 14"x16" CAP
 Cut as shown. No crown if deck is super-elevated.



DETAIL OF FIREWALL
 Scale 3/4" = 1'-0"

OREGON STATE HIGHWAY COMMISSION
DESIGN FOR PILE TRESTLE
26'-0" CONCRETE ROADWAY DECK
2 - 3'-6" SIDEWALKS
 SCALE AS NOTED
 DRAWN BY M.A.L. JANUARY 29, 1935
 DRAWING NO. 5451

Approved: _____
 Bridge Engineer

Revised: April 17, 1932, Sept 7, 1936, Nov 25, 1938