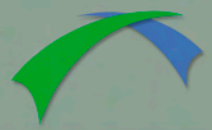
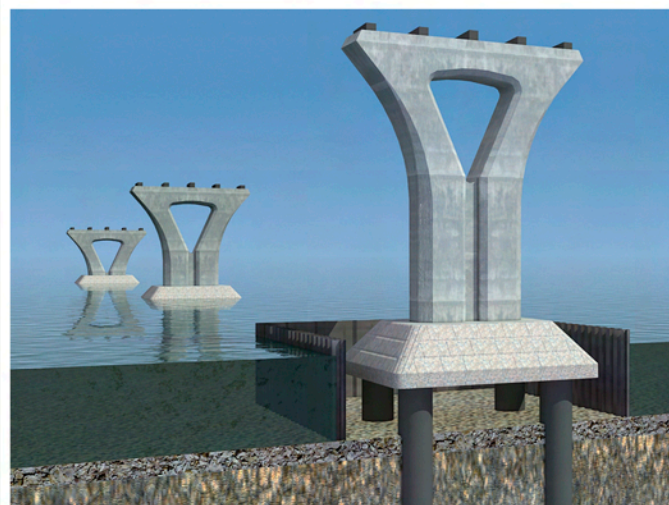




Lake Champlain Bridge Crown Point, NY to Chimney Point, VT



Rendering of New Bridge



The new bridge will be supported by a total of seven piers. Six of these will be founded on reinforced concrete shafts, 6 feet in diameter, extending up to 100 ft though the lake bottom to anchor into the bedrock. The shafts will be topped with reinforced concrete footings up to 10 feet thick, which will in turn support the piers. The sides of the footings are sloped and they will also be clad in granite masonry, which will help protect them from ice damage and complement the historical significance of the region. On the Vermont side of the lake, the depth to bedrock is shallow enough that one of the piers will be founded directly on the rock.

Foundations: Summer - Fall 2010



The roadway over the approach spans will be supported by a superstructure comprising 5 lines of I-shaped steel girders, with height of just over 8 feet. The steel used for the superstructure is a special type of steel known as weathering steel, which is designed to form a thin coat of protective rust. In addition, the steel will be further protected by a process called metalizing which will be used instead of paint. The result will be a low maintenance structure designed with high durability. Once the superstructure steel has been erected, the concrete deck will be cast and the sidewalk and railings installed.

Approach Superstructure: Winter 2010 - Summer 2011

Clients:
New York State
Department of Transportation
Vermont Agency of Transportation

Construction Cost:
\$ 70 million

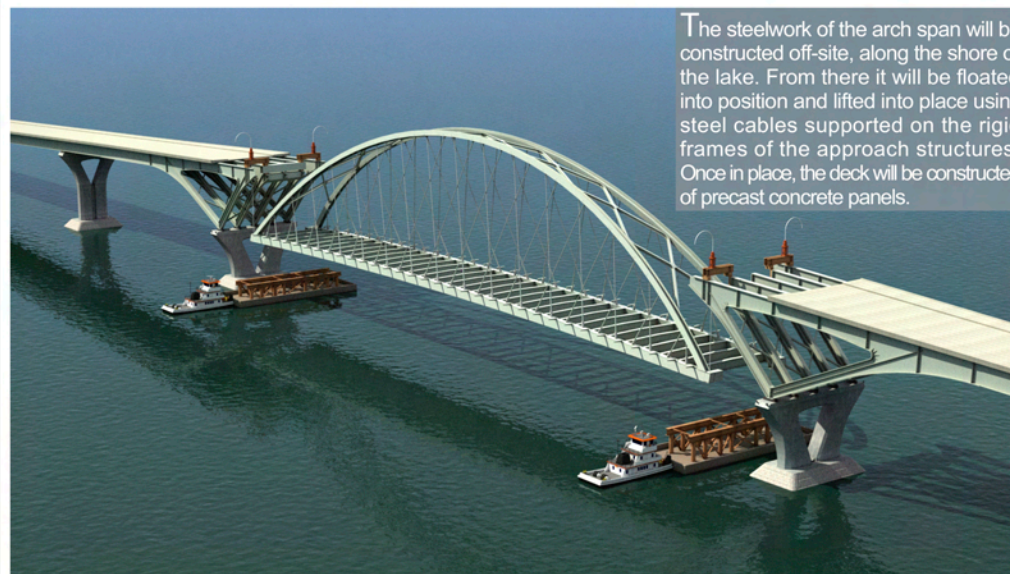
Construction Began:
June 2010

Bridge Open to Traffic:
October 9, 2011 (goal)

Contract Completion:
December 2011



Rendering of New Bridge



The steelwork of the arch span will be constructed off-site, along the shore of the lake. From there it will be floated into position and lifted into place using steel cables supported on the rigid frames of the approach structures. Once in place, the deck will be constructed of precast concrete panels.

Arch Superstructure: Summer 2011



Rendering of New York Approach

The approaches, which include the sidewalks and road leading to the bridge, will be constructed throughout the duration of the project. Once completed, the new bridge will be opened to traffic and the causeways and temporary ferry slips removed.

The inclusion of sidewalks and bicycle accommodations on the bridge, in addition to the selection of this specific bridge design, came about through consultation and collaboration with the people of the region who have a strong understanding of the significance of this crossing.

The New York Approach roadway will remain on the same alignment to avoid impact to the historic Fort St. Frederic and former Champlain Bridge Toll Keeper's House.

In Vermont, the roadway will be shifted approximately 7 feet to the north of the existing alignment, smoothing the approach roadway curve and minimizing impacts to the Chimney Point State Historic Site.



Rendering of Vermont Approach

Approaches: On-going throughout construction

