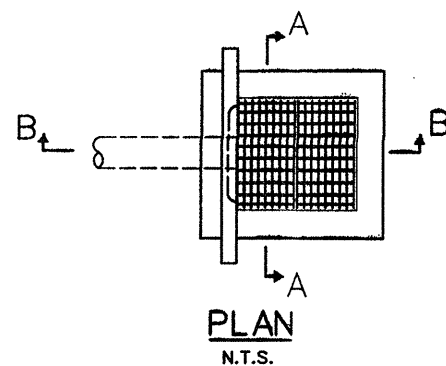
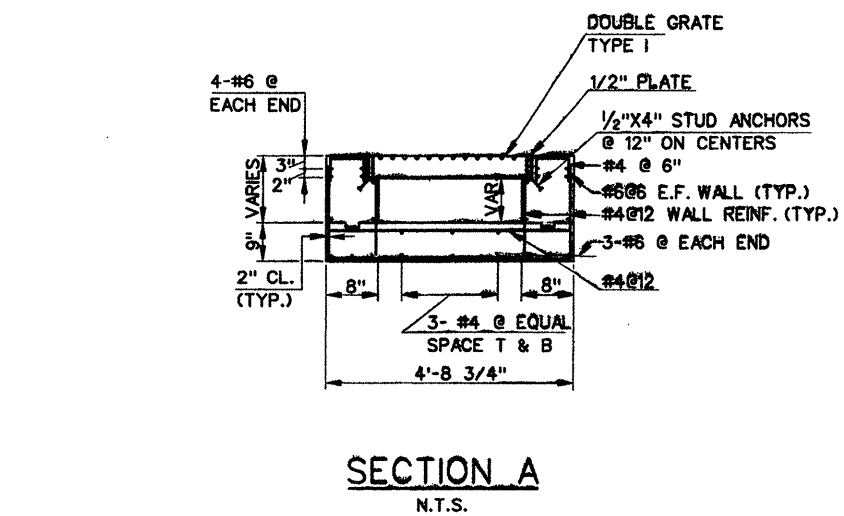
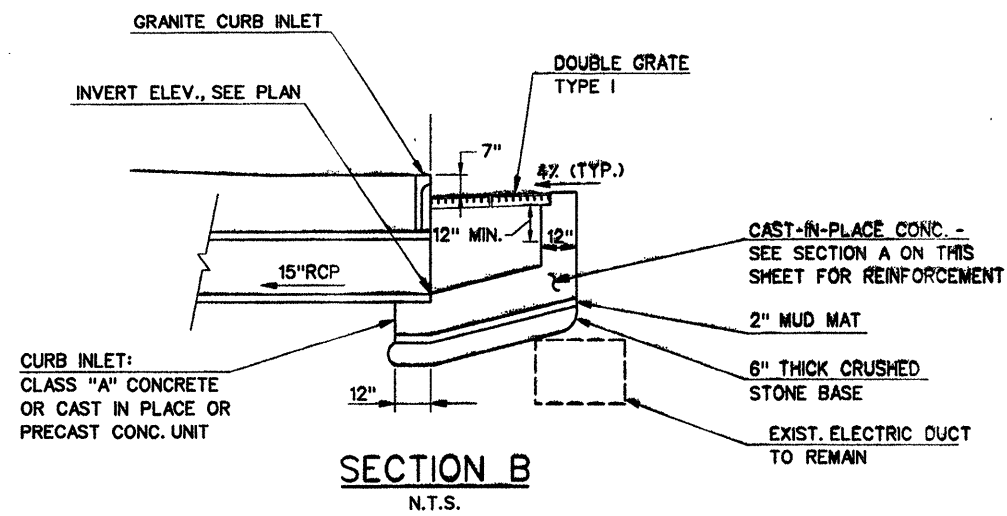




MISCELLANEOUS DRAINAGE DETAILS

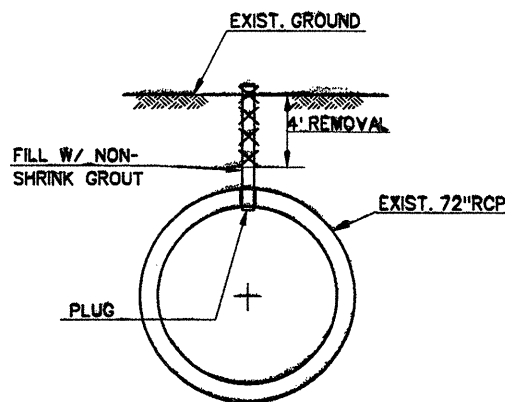
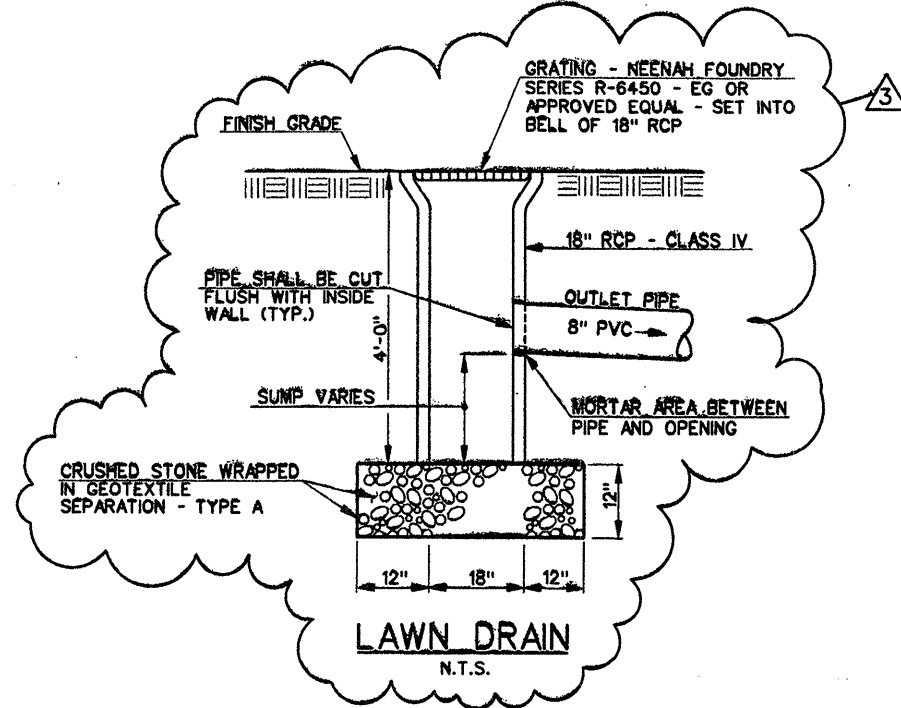
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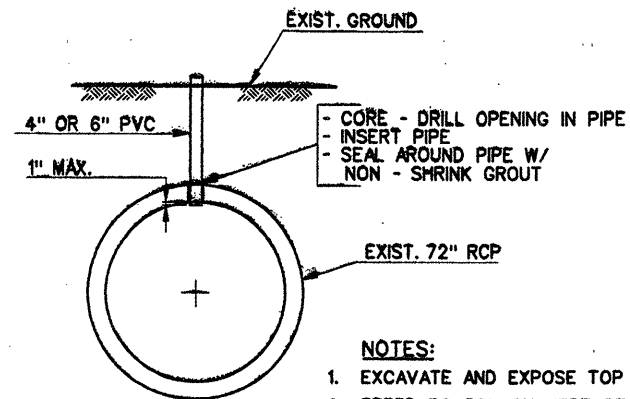
GUTTER INLET

NOTES:

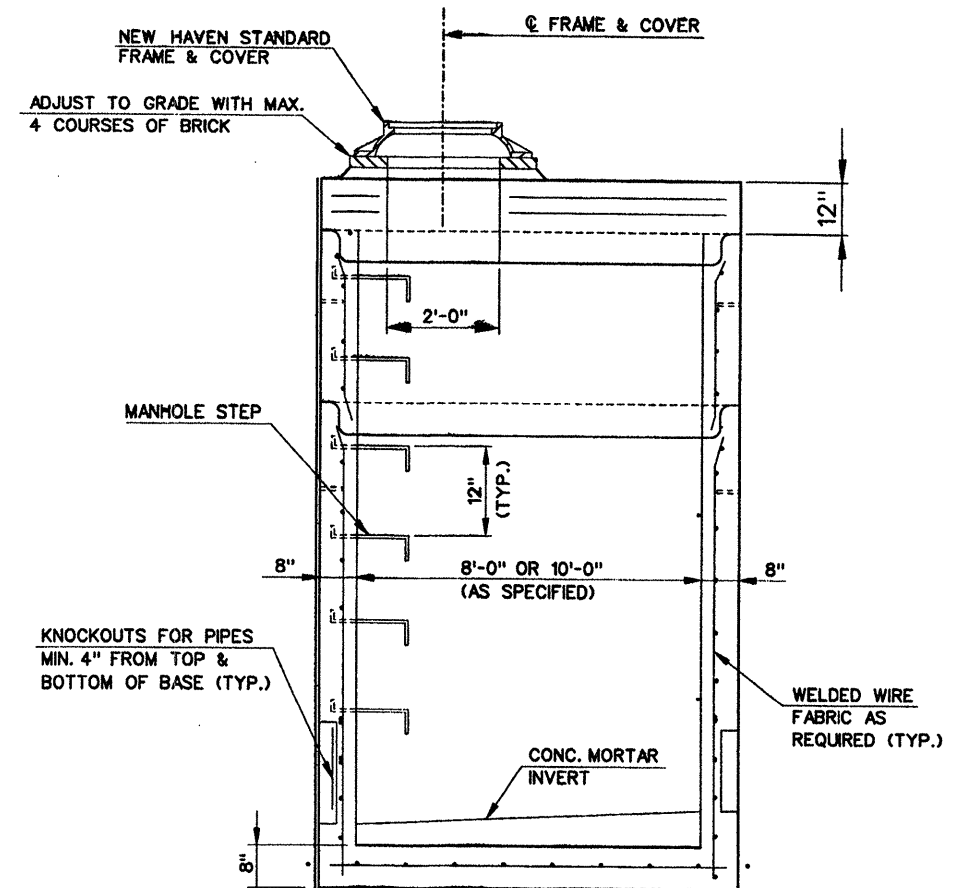
- FOR DETAILS OF CURB INLET, SEE STANDARD DETAILS FOR TYPE "C" CATCH BASIN
- PRIOR TO START OF CONSTRUCTION TAKE A TEST PIT TO CONFIRM LOCATION OF EXIST. ELECTRIC DUCT.
- WALL REINFORCEMENT SHOWN IN SECTION A IS TYPICAL FOR ALL 4 WALLS.



REMOVAL DETAIL



TAPPING 72" RCP FOR BY-PASS PUMPING

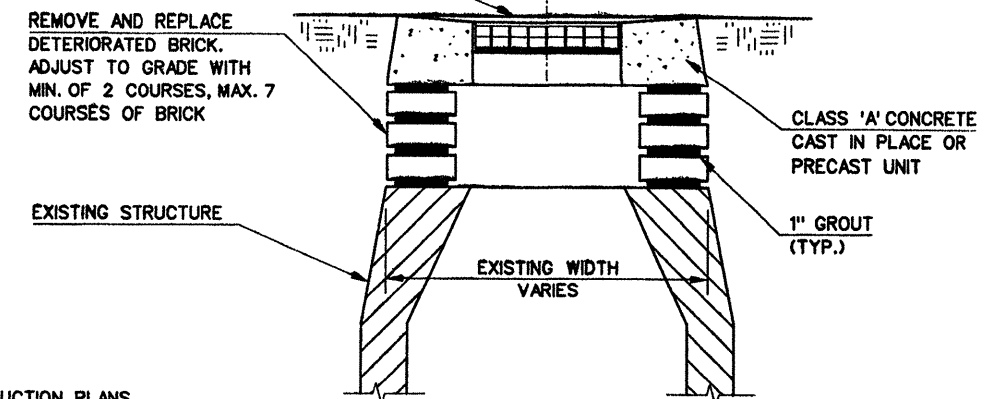


NOTES:

- TOP SLAB OPENINGS 2'-0"
- MANHOLE MANUFACTURED IN ACCORDANCE WITH A.S.T.M. - C - 478.

TYPICAL 8'-0" OR 10'-0" MANHOLE DETAIL

STANDARD TYPE 'C-L' FRAME & GRATE  
SEE STD. SHT. 507-K



TYPE 'C-L' REPLACEMENT COVER

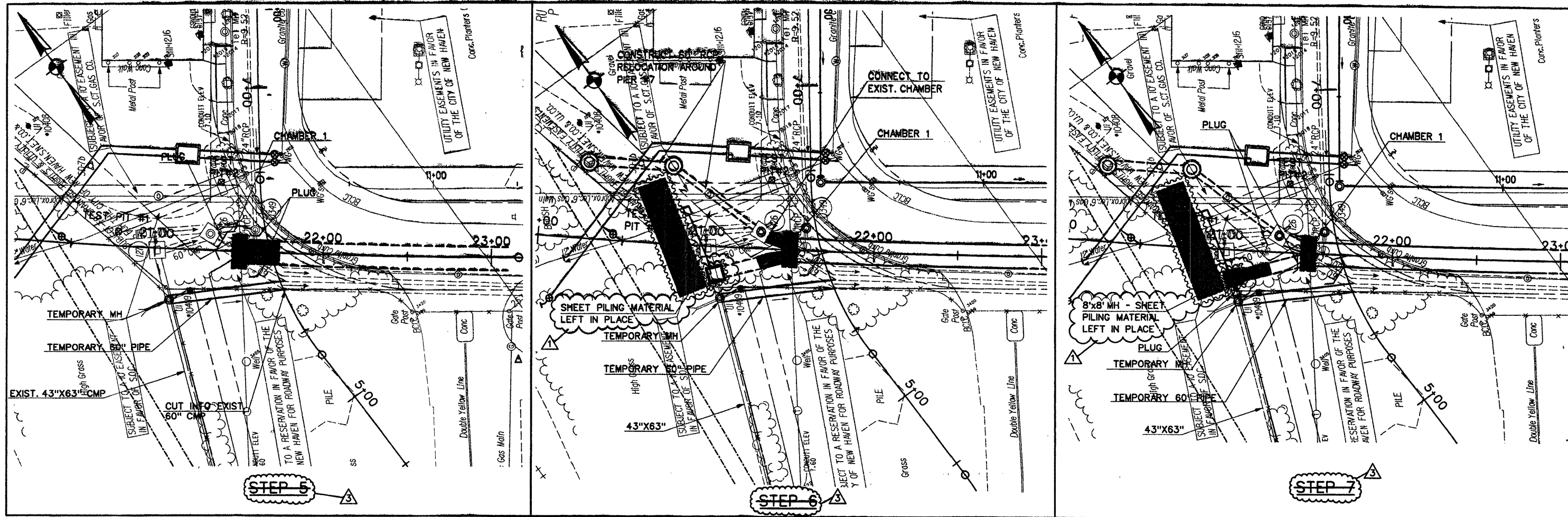
ADDENDUM NO. 3

SCALE AS NOTED		DESIGNER: A. MARGIOTTA	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	PROJECT TITLE: CHURCH STREET SOUTH EXTENSION OVER NEW HAVEN INTERLOCKING AND RAIL YARD	TOWN: NEW HAVEN	PROJECT NO.: 92-526
		DRAFTER: M. OFFENBERG		ENGINEER: PARSONS BRINCKERHOFF QUADE & DOUGLAS, INC.	DRAWING TITLE: MISCELLANEOUS DRAINAGE DETAILS	DRAWING NO.:
		CHECKED BY: A. MORETTI		CADD FILE: AD3R703D013.MDS		SHEET NO.: 23
		DATE CHECKED: 4-9-00		PLOTTED DATE: 11-22-00		

09/03/05 28 NOV 2000 10:48:00 AM C:\pwork\cadd\addendum no. 3\ad3r703d013.mds

# SUGGESTED SEQUENCE OF OPERATIONS

## 4' X 12' PRECAST CONCRETE BOX CULVERT CONSTRUCTION



### STEP 6

**NOTES:**

1. PLUG SOUTHERLY 60" CMP AT MH (126)
2. INSTALL TEMPORARY 60". CUT INTO EXIST. 60" CMP
3. PLUG NORTHERLY 60" AT MH (126). DIVERT FLOW (126) TO (127) TO TEMP. MH
4. PLUG AT MH (349) AND PROVIDE BYPASS PUMP FOR 24" BASE FLOW FROM (349) TO (126)
5. CONSTRUCT JCT. CHAMBER 1
6. CONNECT 24" RCP TO BOX. PLUG BOX AT OUTLET TO JCT. CHAMBER NO. 1
7. REMOVE PLUG AT MH (349) AND REMOVE BYPASS PUMP.

### STEP 7

**NOTES:**

1. CONNECT BOX TO MH (126)
2. CONTINUE LINE TOWARDS 43"x63" CMP
3. CONSTRUCT 60" RCP RELOCATION AROUND PIER #7

### STEP 8

**NOTES:**

1. REMOVE PLUG AT MH (126). DIVERT FLOW FROM 60" PIPE INTO BOX
2. PLUG 60" CMP AT MH (126) AND AT TEMP. MH
3. CONTINUE BOX AND CONSTRUCT 8'x8' MH. SHEET PILING MATERIAL AROUND 8'x8' MH TO BE LEFT IN PLACE, SEE DETAIL SHEET 24.
4. REMOVE TEMP. 60" PIPE
5. STA. 31+20± REMOVE TEMP. 48" PIPE BETWEEN MH (307) AND BOX. PLUG OPENING IN BOX.
6. STA. 31+25± TO STA. 32+75± : REMOVE TEMP. 42" PIPE FROM MH (308) TO NEW MH ON 42" RCP (STA. 32+75± RT.) NEW MH ON 42" RCP WILL REMAIN. PLUG OPENING WHERE TEMP. 42" RCP IS REMOVED.

**GENERAL NOTES:**

1. AT ALL TIMES, MAINTAIN STOCKPILE OF PIPE, CRUSHED STONE & OTHER MATERIALS TO BE USED TO MAINTAIN FLOWS DURING A MAJOR STORM EVENT.

**ADDENDUM NO. 3**

05/04/05 2:20 PM 2000 \\p1p103\cadd\addendum no.3\add3\70310188.hwy

<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 5%;">REV.</td> <td style="width: 15%;">DATE</td> <td style="width: 55%;">DESCRIPTION</td> <td style="width: 25%;">SHEET NO.</td> </tr> <tr> <td>11-22-00</td> <td></td> <td>ADDENDUM NO. 3</td> <td>76</td> </tr> <tr> <td>10-17-00</td> <td></td> <td>ADDENDUM NO. 1</td> <td>76</td> </tr> </table>	REV.	DATE	DESCRIPTION	SHEET NO.	11-22-00		ADDENDUM NO. 3	76	10-17-00		ADDENDUM NO. 1	76	<p>SCALE IN FEET</p> <p>SCALE 1" = 30'</p>	<p>DESIGNER: J. BURCKARDT</p> <p>DRAFTER: M. DEEGAN</p> <p>CHECKED BY: A. MARGIOTTA</p> <p>DATE CHECKED: 4-6-00</p>	<p><b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION</p> <p>ENGINEER: PARSONS BRINCKERHOFF QUAADE &amp; DOUGLAS, INC.</p>	<p>PROJECT TITLE: <b>CHURCH STREET SOUTH EXTENSION OVER NEW HAVEN INTERLOCKING AND RAIL YARD</b></p> <p>CADD FILE: ADIR70310188.HWY</p>	<p>TOWN: <b>NEW HAVEN</b></p> <p>DRAWING TITLE: <b>BOX CULVERT STAGED CONSTRUCTION</b></p>	<p>PROJECT NO.: <b>92-526</b></p> <p>DRAWING NO.: <b>2 OF 2</b></p> <p>SHEET NO.: <b>76</b></p>
REV.	DATE	DESCRIPTION	SHEET NO.															
11-22-00		ADDENDUM NO. 3	76															
10-17-00		ADDENDUM NO. 1	76															

**GENERAL NOTES**

**SPECIFICATIONS:**

CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 814A (1995), SUPPLEMENTAL SPECIFICATIONS DATED JULY 1999 AND SPECIAL PROVISIONS.

**DESIGN SPECIFICATIONS:**

STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES (AASHTO-1996) WITH INTERIM SPECIFICATIONS UP TO AND INCLUDING 1998 AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (1997).

**DESIGN STRESSES:**

CLASS "A" CONCRETE ..... BASED ON f'c = 3,000 psi  
 CLASS "F" CONCRETE ..... BASED ON f'c = 4,000 psi  
 HIGH PERFORMANCE CONCRETE ..... BASED ON f'c = 4,000 psi  
 REINFORCEMENT (ASTM A615 GRADE 60) ..... fy = 60 ksi  
 REINFORCEMENT-CLADDED STAINLESS STEEL (NUOVINOX 316L) ..... fy = 75 ksi  
 STRUCTURAL STEEL (AASHTO M270 GRADE 50) ..... fy = 50 ksi  
 STRUCTURAL STEEL (AASHTO M270 GRADE 50W) ..... fy = 50 ksi  
 STRUCTURAL STEEL (AASHTO M270 GRADE HPS 70W) ..... fy = 70 ksi

**DESIGN METHOD:**

LOAD FACTOR METHOD (SUBSTRUCTURE AND SUPERSTRUCTURE)

**LIVE LOAD:**

HS20-44

**FUTURE PAVING ALLOWANCE:**

30 POUNDS PER SQUARE FOOT.

**BITUMINOUS CONCRETE OVERLAY:**

AT APPROACH SLABS ONLY. THIS SHALL CONSIST OF TWO LIFTS. THE FIRST SHALL BE BITUMINOUS CONCRETE - (2) (1" THICK) AND THE SECOND SHALL BE BITUMINOUS CONCRETE - CLASS (1) (1/2" THICK).

**STRUCTURAL STEEL:**

SEE STRUCTURAL STEEL NOTES FOR DESIGNATIONS AND REQUIREMENTS.

**PAINT-SEGMENTS 1 AND 3:**

PAINTING OF THE STRUCTURAL STEEL IS ONLY REQUIRED AT THE ENDS OF THE GIRDERS. STEEL SURFACES ARE TO BE PREPARED FOR WEATHERING IN ACCORDANCE WITH THE SPECIFICATIONS.

**GALVANIZING-SEGMENT 2:**

ALL STRUCTURAL STEEL SHALL BE GALVANIZED.

**ISOLATION BEARING ASSEMBLIES:**

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE DIMENSIONS OF THE ISOLATION BEARINGS DETAILED ON THE CONTRACT PLANS ARE OF A CONCEPTUAL NATURE. ANY CHANGE IN BEARING HEIGHT RESULTING FROM THEIR DESIGN WILL REQUIRE ADJUSTMENTS TO THE CONCRETE BEARING PAD ELEVATIONS BY THE CONTRACTOR. SEE SPECIAL PROVISIONS.

**FOUNDATION PRESSURES AND PILE LOADS:**

THE VARIOUS GROUP LOADINGS NOTED ON THE SUBSTRUCTURE PLAN SHEETS REFER TO THE GROUP LOADS AS GIVEN IN THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.

**DIMENSIONS:**

ALL DIMENSIONS SHOWN ON THE PLANS ARE GIVEN IN U.S. CUSTOMARY UNITS. ALL ELEVATIONS ARE GIVEN IN FEET. WHEN DIMENSIONS ARE GIVEN TO LESS THAN THREE DECIMAL PLACES, THE OMITTED DIGITS SHALL BE ASSUMED TO BE ZERO.

**FORMER ROUNDHOUSE:**

THE CONTRACTOR SHALL BE AWARE THAT THE FORMER ROUNDHOUSE AREA SHOWN ON DWG. NO. STR-2 HAS BEEN DESIGNATED AS AN ARCHAEOLOGICAL SITE. ANY EXCAVATION WITHIN THIS AREA MUST FOLLOW THE PROCEDURE AS STATED IN THE CONDOT STANDARD SPECIFICATION 814A SECTION 1.10.06 AND IN THE SPECIAL PROVISIONS.

**EXISTING CONDITIONS:**

EXISTING CONDITIONS ARE BASED ON SURVEY PERFORMED SEPTEMBER 1998 AND UPDATED DECEMBER 1998, OCTOBER 1999, JANUARY 2000 AND FEBRUARY 2000.

**RAILROAD COORDINATION:**

THE CONTRACTOR SHALL COMPLETELY COORDINATE HIS OPERATIONS WITHIN THE NEW HAVEN RAIL YARD WITH METRO-NORTH RAILROAD, AMTRAK AND THE STATE OF CONNECTICUT, AS REQUIRED. FOR DETAILS, SEE ELSEWHERE ON THESE PLANS AND IN THE SPECIALS PROVISIONS.

THE CONTRACTOR SHALL HAVE ALL EMPLOYEES AND SUBCONTRACTORS ATTEND THE AMTRAK AND METRO-NORTH RAILROAD SAFETY TRAINING COURSES PRIOR TO COMMENCING ANY WORK WITHIN THE RAIL YARD. ANY EMPLOYEE/SUBCONTRACTOR WHO HAS NOT COMPLETED THE SAFETY TRAINING COURSES WILL BE EXCLUDED FROM ALL WORK WITHIN THE RAIL YARD.

ACCESS TO ALL DRIVEWAYS, PARKING AREAS AND LOADING ZONES SHALL BE MAINTAINED AT ALL TIMES UNLESS APPROVED BY AMTRAK AND/OR METRO-NORTH RAILROAD, AS APPLICABLE.

THE CONTRACTOR IS RESPONSIBLE FOR SCHEDULING RAILROAD FLAGMEN AND GROUND MEN WITH AMTRAK AND/OR METRO-NORTH RAILROAD, AS APPLICABLE FOR PERFORMING WORK ON AND ADJACENT TO THE RAILROAD RIGHT-OF-WAY.

THE CONTRACTOR SHALL SUBMIT TRACK CLOSURE REQUESTS TO THE ENGINEER AT LEAST 14 DAYS IN ADVANCE. THE CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL FROM THE ENGINEER PRIOR TO THE CLOSURE OF ANY TRACK. THE WRITTEN APPROVAL WILL INCLUDE THE TRACK CLOSURE DATE AND CLOSURE TIME, AND SUBSEQUENT TRACK RE-OPENING DATE AND TIME.

THE CONTRACTOR SHALL REQUEST TEMPORARY GRADE CROSSINGS WHENEVER HE NEEDS TO CROSS TRACKS TO PERFORM HIS WORK. THESE CROSSINGS WILL NOT BE MEASURED FOR PAYMENT. THE CONTRACTOR SHALL MAKE REQUEST TO THE STATE FOR TEMPORARY CROSSINGS IN WRITING. THE NEED FOR A TEMPORARY CROSSING WILL BE DETERMINED BY THE STATE AND METRO-NORTH / AMTRAK RAILROAD. IF A TEMPORARY CROSSING IS FOUND TO BE NEEDED, THE CROSSING WILL BE PROVIDED, INSTALLED AND REMOVED BY THE GOVERNING RAILROAD AT NO EXPENSE TO THE CONTRACTOR. AMTRAK AND/OR METRO-NORTH RAILROAD APPROVAL IS REQUIRED FOR LOCATIONS OF ALL TEMPORARY CROSSINGS, AS WELL AS ALL ROUTES THE CONTRACTOR PROPOSES TO COMPLETE HIS OPERATIONS. SPECIAL ATTENTION SHALL BE GIVEN TO ROUTES UNDER CATENARY WIRES.

**ENVIRONMENTAL:**

THE ENTIRE PROJECT AREA IS CONSIDERED AN "AREA OF ENVIRONMENTAL CONCERN". SEE ROADWAY DRAWINGS AND THE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL NOTE THAT ALL DEWATERING EFFLUENT SHALL BE CONVEYED TO THE GROUNDWATER TREATMENT SYSTEM AREA. SEE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL BE AWARE THAT ALL EXCAVATED MATERIALS AND ALL REMOVED EXISTING MASONRY AND RAILROAD TIES MUST BE TAKEN TO THE WASTE STOCKPILE AREA FOR TESTING. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO AS NOT TO GENERATE MORE MATERIAL THAN THE WASTE STOCKPILE AREA CAN ACCOMMODATE. THE CONTRACTOR SHALL ALLOW SUFFICIENT TIME FOR THE COMPLETE SAMPLING AND TESTING, INCLUDING OBTAINING TEST REPORTS, OF THE MATERIAL DELIVERED TO THE WASTE STOCKPILE AREA. THE CONTRACTOR SHALL NOTE THAT IT IS ANTICIPATED THAT THE RE-USE OF EXCAVATED MATERIAL AS BACKFILL, ETC. MAY BE ALLOWED. SEE ROADWAY PLANS AND THE SPECIAL PROVISIONS.

S.H.G.W. - SEASONAL HIGH GROUNDWATER ELEVATION

**NEW HAVEN RAIL YARD PROJECTS:**

THE CONTRACTOR IS MADE AWARE THAT SEVERAL NEW HAVEN RAIL YARD PROJECTS WILL BE CONSTRUCTED WITHIN THE SAME TIME FRAME AND WITHIN THE PROJECT LIMITS OF STATE PROJECT NO. 92-526. THOSE PROJECTS INCLUDE BUT ARE NOT NECESSARILY LIMITED TO THE FOLLOWING:

- STATE PROJECT NO. 301-0001, "NEW HAVEN INTERLOCKING RECONFIGURATION"
- STATE PROJECT NO. 301-0039, "NEW HAVEN RAIL YARD COMPLEX FACILITIES IMPROVEMENTS"
- AMTRAK PROJECT RFP NO. UGJP 0026, "LOCOMOTIVE SHOP, OFFICE AND MATERIAL CONTROL BUILDING"
- STATE PROJECT NO. 301-0025, "PLAN FOR CATENARY REPLACEMENT BETWEEN STRUCTURES 1045 AND 73-16(AM)"

THE CONTRACTOR SHALL COMPLETELY COORDINATE HIS OPERATIONS WITH THESE PROJECTS.

**TRACK DESIGNATION:**

PARCEL 'G' TRACKS: TRACK 11 AND ALL TRACKS NORTH OF TRACK 11  
 MAINLINE TRACKS: TRACK 3 SOUTH TO TRACK 10  
 YARD TRACKS: INBOUND TRACK AND ALL TRACKS SOUTH OF INBOUND TRACK

**CONCRETE NOTES**

**REMAIN-IN-PLACE FORMS:**

THE USE OF REMAIN-IN-PLACE FORMS IS REQUIRED FOR SPANS OVER ELECTRIFIED RAIL LINES. REMAIN-IN-PLACE FORMS SHALL BE USED AT SPANS 2 AND 5. THE GIRDERS, STRINGERS, FLOOR BEAMS AND THE TRUSS HAVE BEEN DESIGNED FOR THE ADDITIONAL WEIGHT OF 15 PSF FOR THE REMAIN-IN-PLACE FORMS. THE USE OF REMAIN-IN-PLACE FORMS WILL NOT BE ALLOWED ELSEWHERE ON THE STRUCTURE.

**COMPOSITE CONSTRUCTION:**

NO TEMPORARY INTERMEDIATE SUPPORTS SHALL BE USED DURING THE PLACING AND SETTING OF THE CONCRETE DECK SLAB. TEMPORARY SUPPORTS MAY ONLY BE USED FOR STRUCTURAL STEEL ERECTION ONLY AND TEMPORARY SUPPORTS SHALL NOT BE USED BETWEEN PIERS 1 AND 2. CONSTRUCTION LOADS AND DEAD LOADS WILL BE PERMITTED WHEN DIRECTED BY THE ENGINEER BUT ONLY WHEN THE CONCRETE HAS REACHED A STRENGTH OF f'c = 3,500 psi. LIVE LOADS (TRAFFIC) WILL BE PERMITTED ON THE STRUCTURE AFTER THE CONCRETE HAS REACHED A STRENGTH OF f'c = 4,000 psi.

**CLASS "A" CONCRETE:**

CLASS "A" CONCRETE SHALL BE USED FOR THE ENTIRE SUBSTRUCTURE AND THE PARAPETS OF U-TYPE WINGS WITH THE EXCEPTION OF THE CLASS "F" CONCRETE USED IN THE PIER WALLS AND BEARING PADS.

**CLASS "F" CONCRETE:**

CLASS "F" CONCRETE SHALL BE USED FOR PIER WALLS, BEARING PADS AND APPROACH SLABS.

**HIGH PERFORMANCE CONCRETE:**

HIGH PERFORMANCE CONCRETE SHALL BE USED FOR BRIDGE DECKS, INCLUDING SIDEWALKS AND PARAPETS.

**JOINT SEAL:**

SEE SPECIAL PROVISIONS.

**EXPOSED EDGES:**

EXPOSED EDGES OF CONCRETE SHALL BE BEVELED 1" x 1" UNLESS DIMENSIONED OTHERWISE.

**CONCRETE COVER:**

ALL REINFORCEMENT SHALL HAVE 2" COVER UNLESS DIMENSIONED OTHERWISE.

**REINFORCEMENT:**

ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60 UNLESS NOTED AS CLADDED STAINLESS STEEL, IN WHICH CASE IT SHALL BE NUOVINOX 316L CLADDED STAINLESS STEEL.

**CLADDED STAINLESS STEEL REINFORCING BARS:**

ALL REINFORCEMENT IN THE SUPERSTRUCTURE INCLUDING THE CONCRETE DECK SLAB, SIDEWALK AND PARAPETS SHALL BE CLADDED STAINLESS STEEL UNLESS OTHERWISE NOTED. THESE BARS SHALL BE INCLUDED IN THE ITEM "DEFORMED STEEL BARS (CLADDED STAINLESS STEEL)".

**EPOXY COATED REINFORCING BARS:**

ALL REINFORCEMENT IN THE CONCRETE APPROACH SLABS, INCLUDING THOSE IN THE HEADERS, SHALL BE EPOXY COATED. THESE BARS SHALL BE INCLUDED IN THE ITEM "DEFORMED STEEL BARS (EPOXY COATED)".

**PREFORMED EXPANSION JOINT FILLER:**

THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE ITEM "CLASS 'A' CONCRETE".

**CLOSED CELL ELASTOMER:**

THE COST OF FURNISHING AND INSTALLING CLOSED CELL ELASTOMER SHALL BE INCLUDED IN THE COST OF THE ITEM "CLASS 'A' CONCRETE".

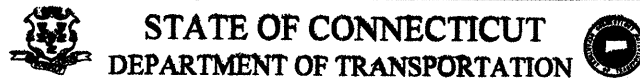
**CONSTRUCTION JOINTS:**

CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS, WILL NOT BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.

**STAIN PROTECTION:**

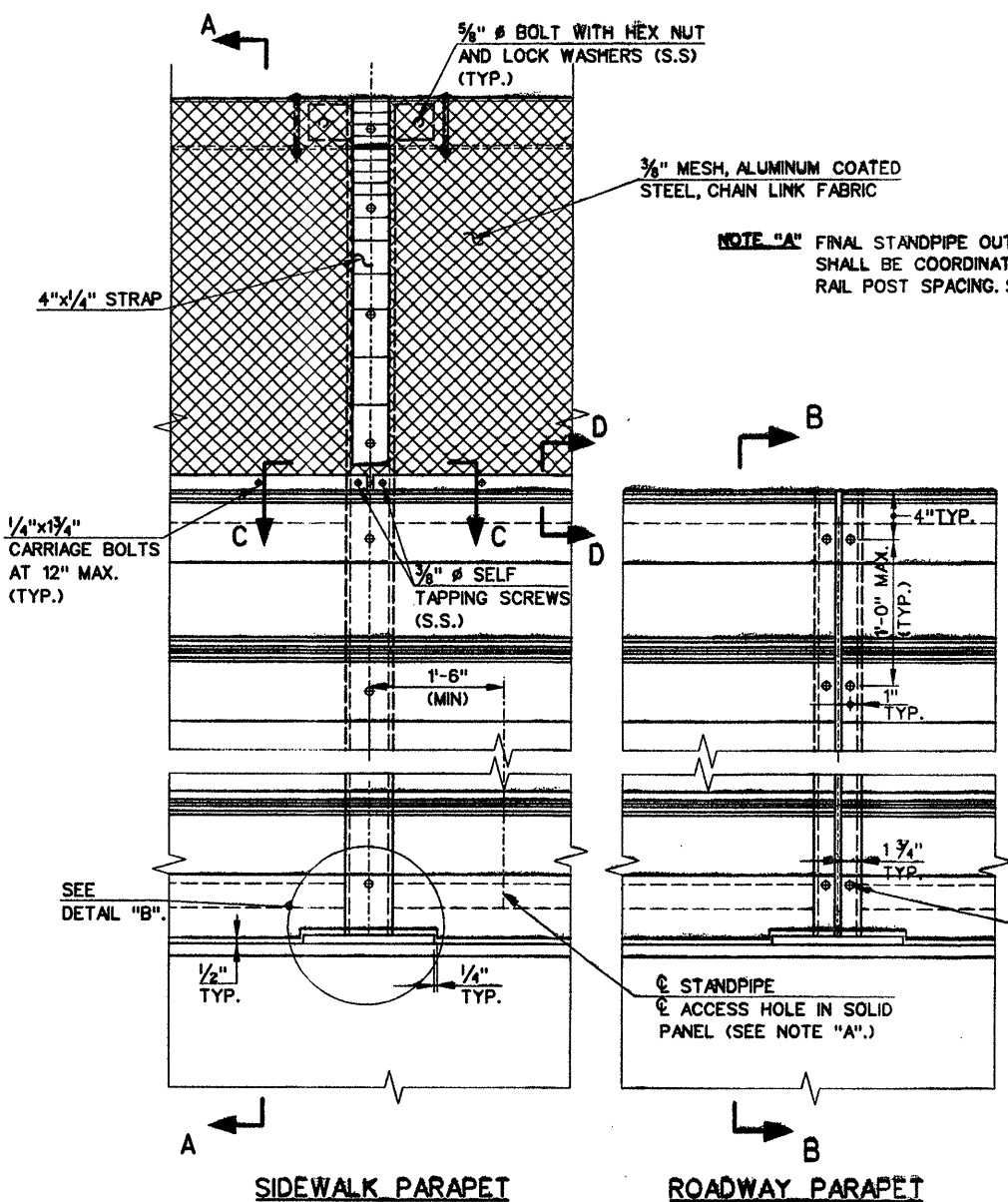
POLYETHYLENE FILM SHALL BE USED TO PROTECT THE ABUTMENTS AND PIER STEMS FROM SUPERSTRUCTURE STAINING AND SHALL EXTEND FROM THE TOP OF THE STEMS TO THE TOP OF THE FOOTINGS. THE FILM SHALL REMAIN IN PLACE UNTIL AFTER THE BRIDGE DECK HAS BEEN PLACED. SEE SPECIAL PROVISION "STAIN PROTECTION".

**ADDENDUM NO. 3**

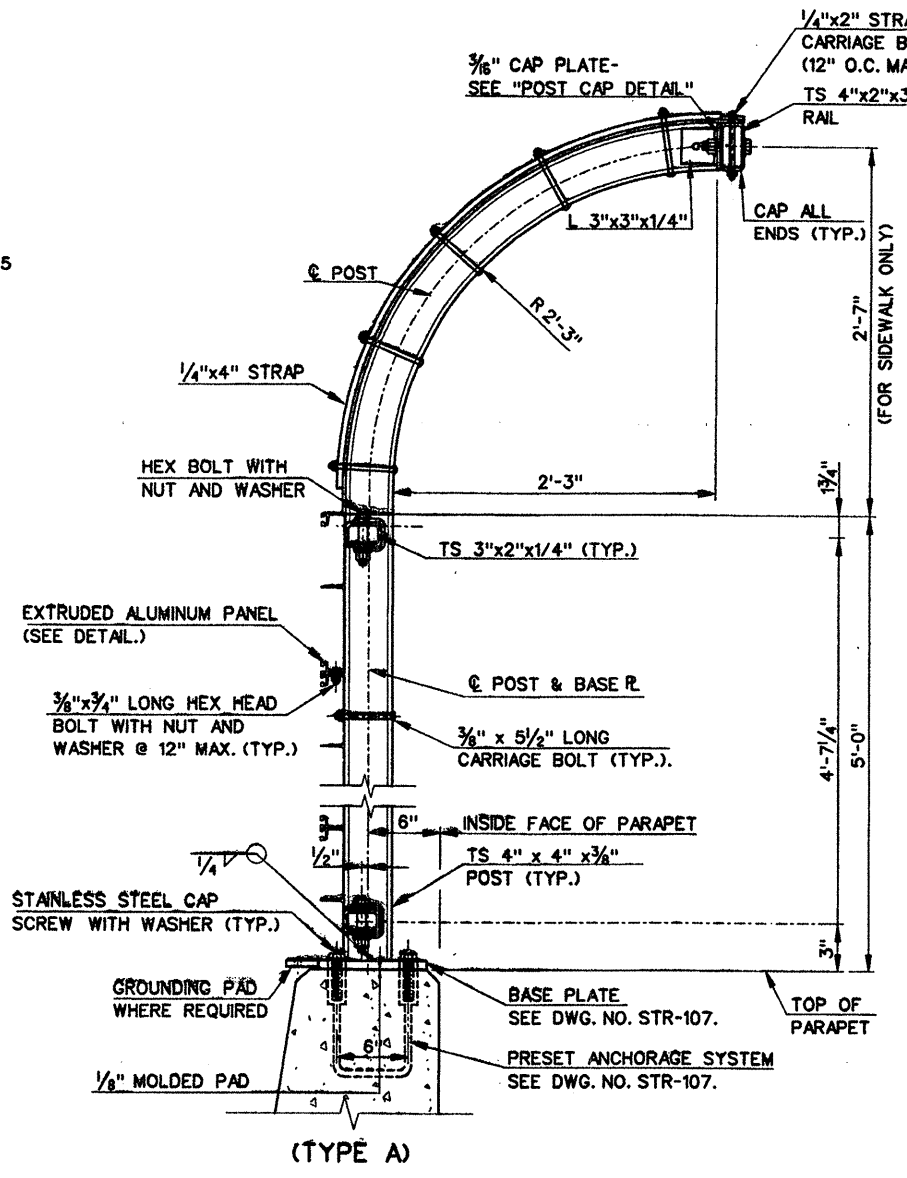
DESIGNER: D. GEISSERT DRAFTER: D. GEISSERT CHECKED BY: A. MORETTI DATE CHECKED: 4-8-00		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION		PROJECT TITLE: CHURCH STREET SOUTH EXTENSION OVER NEW HAVEN INTERLOCKING AND RAIL YARD		TOWN: NEW HAVEN		PROJECT NO.: 92-526	
ENGINEER: PARSONS BRINCKERHOFF QUADE & DOUGLAS, INC. APPROVED BY: <i>Anthony A. Morretti</i>		DATE: 11/28/00		DRAWING TITLE: GENERAL NOTES		DRAWING NO.: STR-4		SHEET NO.: 138	
CADD FILE: AD3R703S003.DGN		PLOTTED DATE: 11-27-00							

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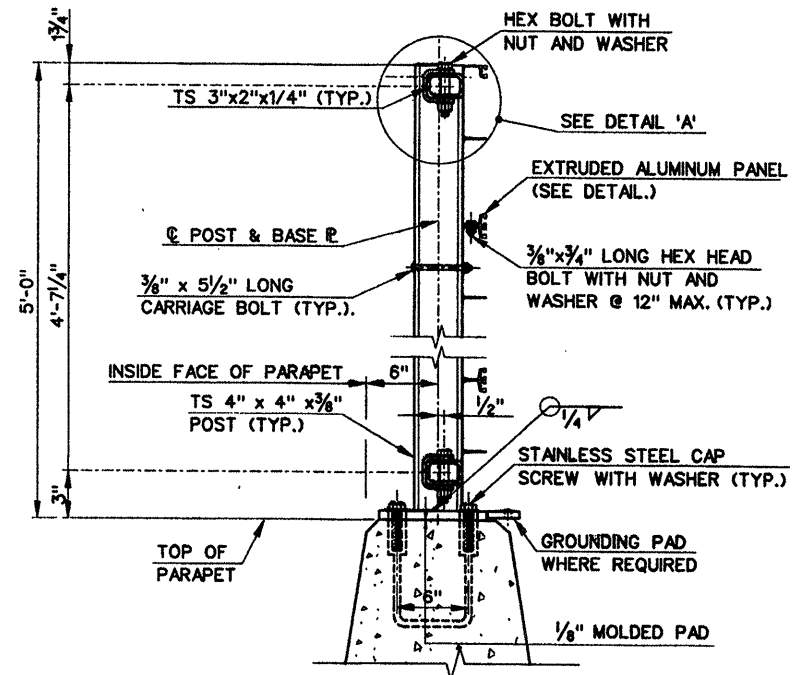
REV.	DATE	DESCRIPTION	SHEET NO.
11-22-00		ADDENDUM NO. 3 - TRACK CROSSING NOTE	138



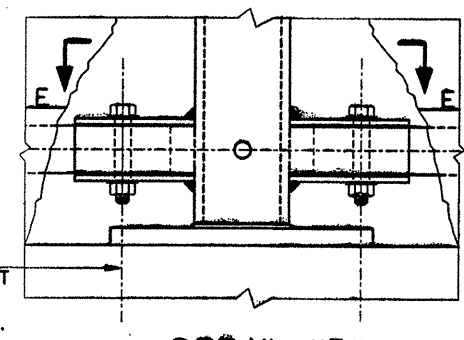
**SIDEWALK PARAPET**  
**ROADWAY PARAPET**  
(LOOKING AT OUTSIDE OF PARAPETS)  
**ELEVATIONS**  
SCALE: 1/2" = 1'-0"



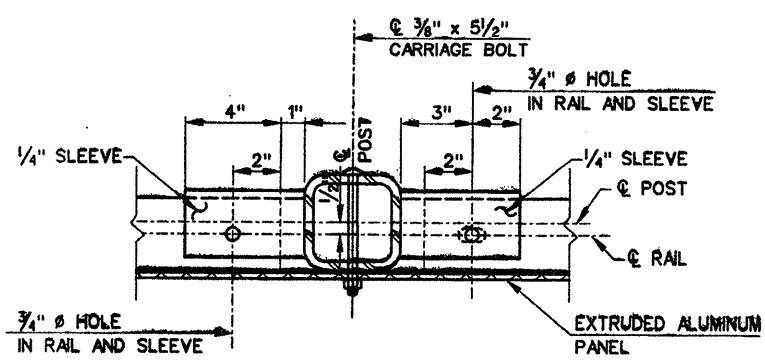
**(TYPE A)**  
**SECTION A-A**  
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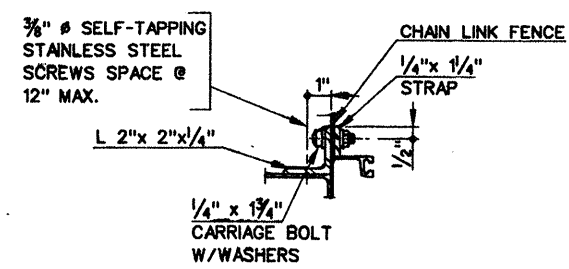
**(TYPE B)**  
**SECTION B-B**  
SCALE: 1/2" = 1'-0"



**DETAIL "B"**  
SCALE: 3" = 1'-0"



**SECTION E-E**  
SCALE: 3" = 1'-0"



**SECTION D-D**  
SCALE: 3" = 1'-0"

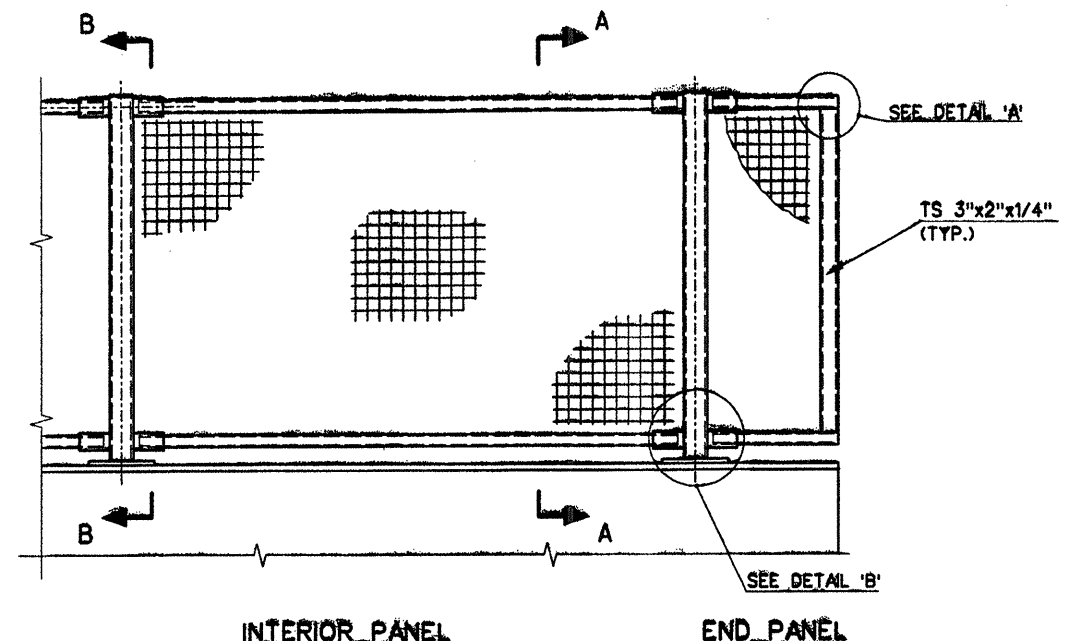
**NOTES**

1. THE POSTS, RAILS, BASES, SLEEVES AND CONNECTION MATERIAL SHALL BE EXTRUDED ALUMINUM CONFORMING TO ASTM B221, ALUMINUM ALLOY 6061-T6 AND SHALL BE ANODIZED (CLEAR) SEE SPECIAL PROVISION.
2. ALL CAP SCREWS SHALL BE STAINLESS STEEL AND CONFORM TO THE REQUIREMENTS OF ASTM A193, CLASS 1 OR 2, GRADE B8M (ASITYPE 316). STAINLESS STEEL WASHERS SHALL CONFORM TO ASTM A240, TYPES 302-305.
3. FOR RAIL POST SPACING SEE SLAB PLANS.
4. THE RAILING SHALL BE FABRICATED AND ERECTED SO THAT THE EXTRUDED ALUMINUM PANELS ARE PARALLEL TO EACH OTHER AND TO THE PARAPET AND THE RAILING POSTS ARE VERTICAL.
5. EXPOSED SURFACES OF POSTS, RAILING, CONNECTION PLATES AND ANGLES AND RAILING PANELS SHALL BE FREE OF BURRS, IRREGULARITIES AND SHARP EDGES.
6. FOR CONNECTION TO THE GROUNDING AND BONDING SYSTEM, SEE DWG. NO. STR-118. GROUNDING PADS ARE TO BE PAID FOR UNDER THE APPLICABLE METAL BRIDGE RAIL (SOLID PANEL) ITEM.
7. FOR SECTIONS C-C AND DETAIL 'A'. SEE DWG. NO. STR-106.
8. METAL BRIDGE RAIL MOUNTED ON THE SIDEWALK PARAPET SHALL BE PAID FOR UNDER THE "METAL BRIDGE RAIL (SOLID PANEL) (TYPE A)".
9. METAL BRIDGE RAIL MOUNTED ON THE ROADWAY PARAPET SHALL BE PAID FOR UNDER THE ITEM "METAL BRIDGE RAIL (SOLID PANEL) (TYPE B)".

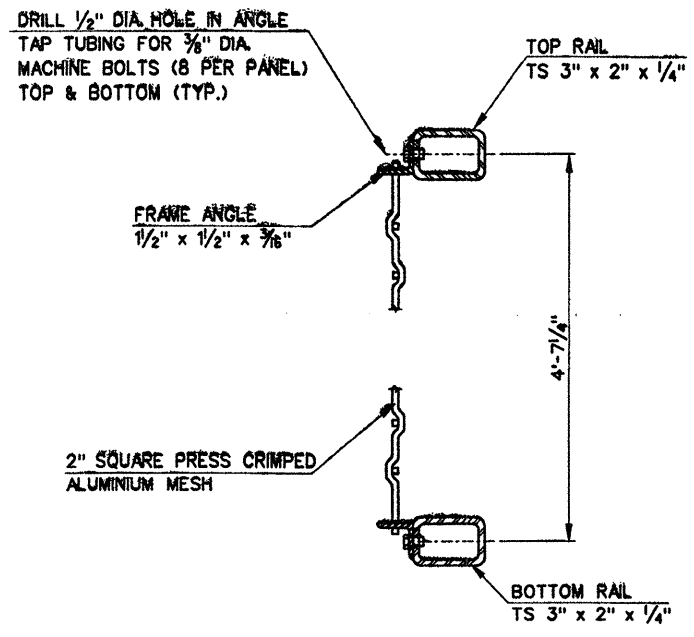
**ADDENDUM NO. 3**

<p>DESIGNER: D. CICHOWSKI          DRAFTER: A. KILPATRICK          CHECKED BY: A. MORETTI          DATE CHECKED: 09-00</p>			<p><b>STATE OF CONNECTICUT</b>  <b>DEPARTMENT OF TRANSPORTATION</b></p> <p>ENGINEER: PARSONS, BRINCKERHOFF, QUAADE &amp; DOUGLAS, INC.          APPROVED BY: <i>Anthony A. Moretti</i> DATE: 11/28/00</p>		<p>PROJECT TITLE: CHURCH STREET SOUTH EXTENSION          OVER NEW HAVEN INTERLOCKING          AND RAIL YARD</p> <p>TOWN: NEW HAVEN          PROJECT NO.: 92-526          DRAWING NO.: STR-105          SHEET NO.: 239</p>			
<p>SCALE AS NOTED</p>			<p>ADDENDUM NO. 3 - BOLT MATERIAL          ADDENDUM NO. 2 - BRIDGE RAIL COLOR CHANGED</p>			<p>CADD FILE: AD3R703S135.DGN          PLOTTED DATE: 11-27-00</p>		

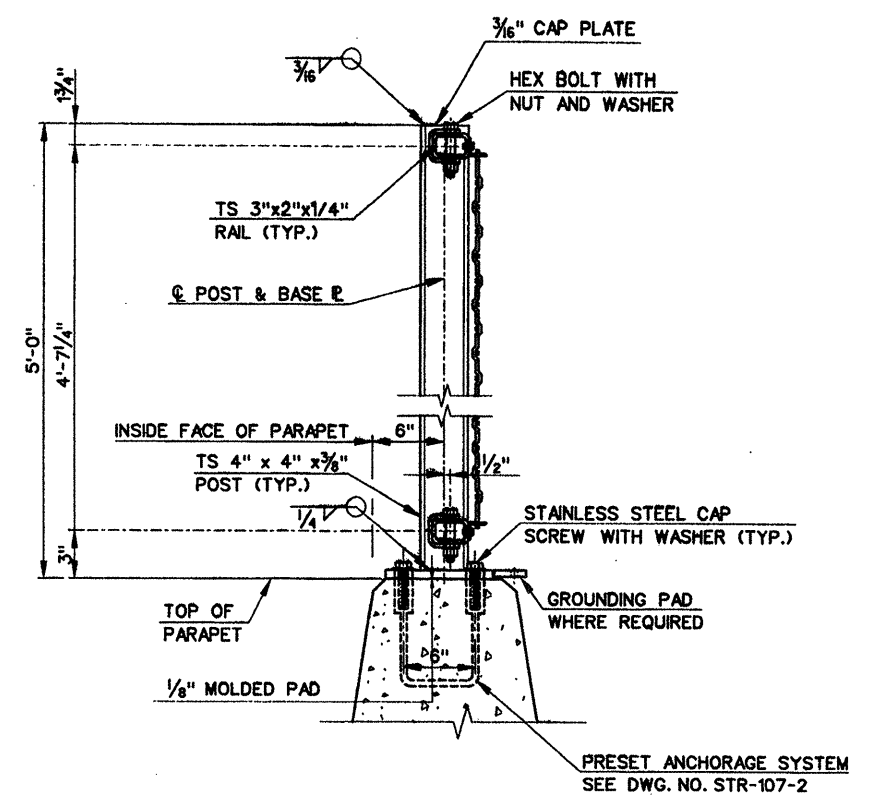
05/02/05 28 NOV 2000  
 A:\m\ad3r703\ad3r703.dgn



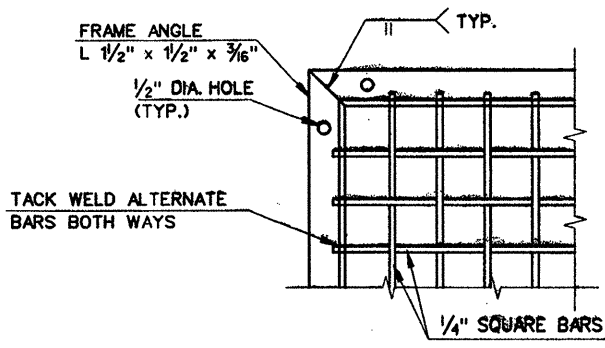
**INTERIOR PANEL**  
**END PANEL**  
**(LOOKING AT INSIDE OF PARAPETS)**  
**ELEVATION**  
SCALE: 3/4" = 1'-0"



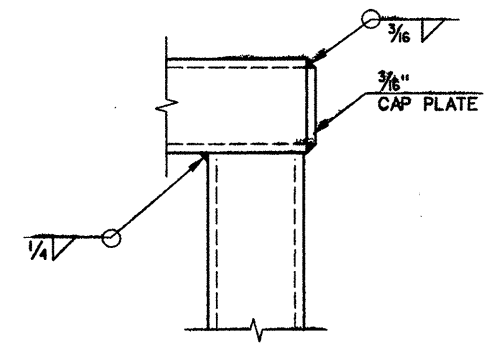
**SECTION A-A**  
SCALE: 3"-1'-0"



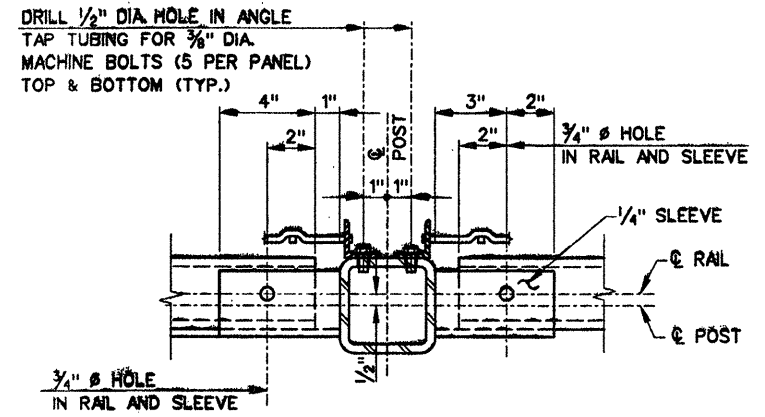
**SECTION B-B**  
SCALE: 1/2"-1'-0"



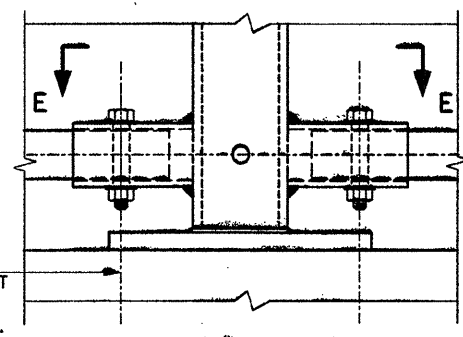
**CORNER DETAIL**  
SCALE: 3"-1'-0"



**DETAIL 'A'**  
SCALE: 3"-1'-0"



**SECTION E-E**  
SCALE: 3"-1'-0"



**DETAIL 'B'**  
SCALE: 3"-1'-0"

**NOTES**

1. THE POSTS, RAILS, BASES, SLEEVES AND CONNECTION MATERIAL SHALL BE EXTRUDED ALUMINUM CONFORMING TO ASTM B221, ALUMINUM ALLOY 6061-T6 AND SHALL BE ANODIZED (CLEAR) SEE SPECIAL PROVISION.
2. ALL CAP SCREWS SHALL BE STAINLESS STEEL AND CONFORM TO THE REQUIREMENTS OF ASTM A193, CLASS 1 OR 2, GRADE B8M (AISI TYPE 316). STAINLESS STEEL WASHERS SHALL CONFORM TO ASTM A240, TYPES 302-305.
3. FOR RAIL POST SPACING SEE SLAB PLANS.
4. THE RAILING SHALL BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE PARAPET AND THE RAILING POSTS ARE VERTICAL.
5. EXPOSED SURFACES OF POSTS, RAILING, CONNECTION SLEEVES, ANGLES AND RAILING PANELS SHALL BE FREE OF BURRS, IRREGULARITIES AND SHARP EDGES.
6. FOR CONNECTION TO THE GROUNDING AND BONDING SYSTEM, SEE DWG. NO. STR-118. GROUNDING PADS ARE TO BE PAID FOR UNDER THE ITEM METAL BRIDGE RAIL-PROTECTIVE FENCE (TYPE C).

**ADDENDUM NO. 3**

09/04/00 08:44 AM 241-1 STR-107-1.dwg

REV.	DATE	DESCRIPTION	SHEET NO.
11-22-2000		ADDENDUM NO. 3 - BOLT MATERIAL	241-1
11-7-2000		ADDENDUM NO. 2 - BRIDGE RAIL COLOR CHANGED	241-1
		REVISIONS	

SCALE AS NOTED

DESIGNER: D. CICHOWSKI  
 DRAFTER: A. KILPATRICK  
 CHECKED BY: A. MORETTI  
 DATE CHECKED: 4-9-00

**STATE OF CONNECTICUT**  
**DEPARTMENT OF TRANSPORTATION**

ENGINEER: PARSONS BRINCKERHOFF QUADE & DOUGLAS, INC.  
 APPROVED BY: *Anthony D. Moretti* DATE: 11/27/00

PROJECT TITLE:  
**CHURCH STREET SOUTH EXTENSION  
 OVER NEW HAVEN INTERLOCKING  
 AND RAIL YARD**

CADD FILE: AD3R703S138.DGN PLOTTED DATE: 11-27-00

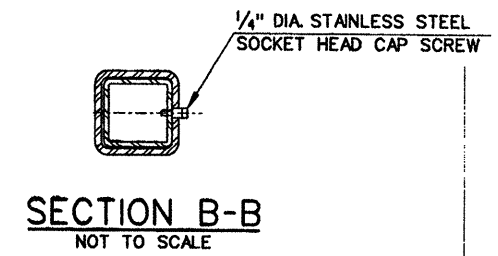
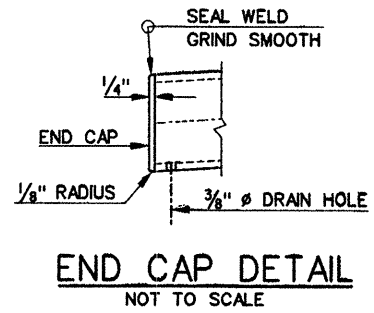
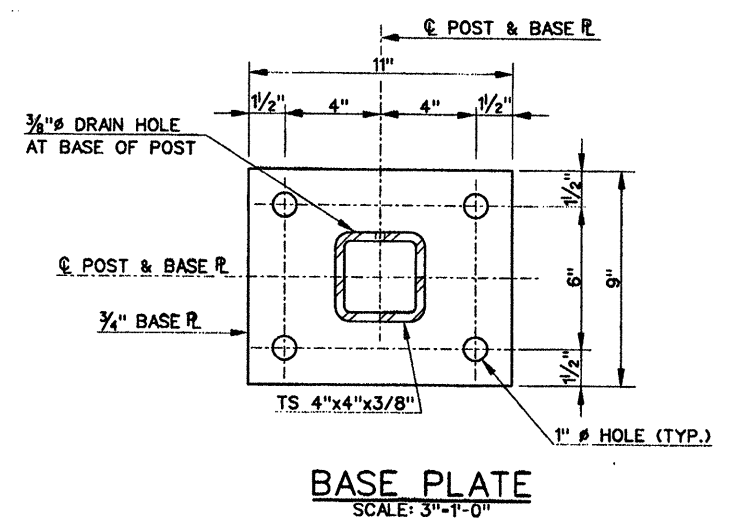
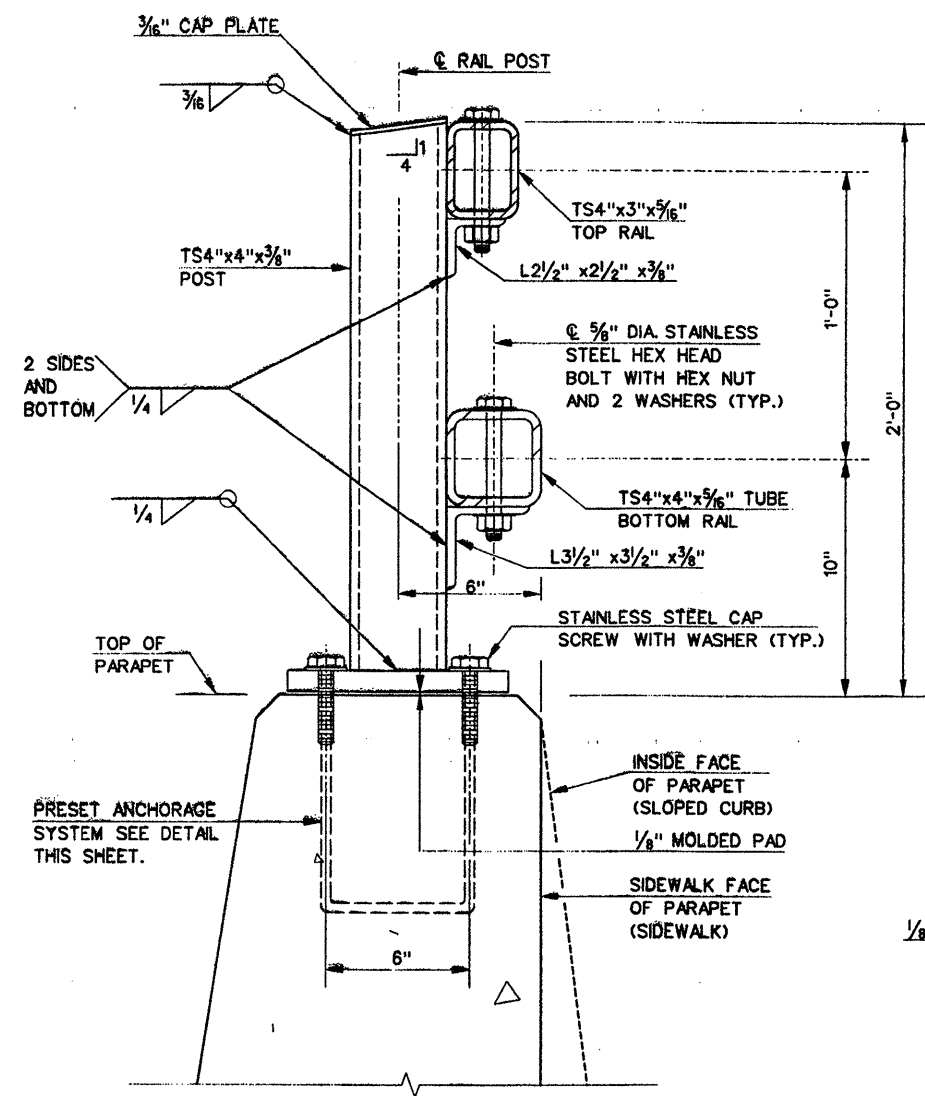
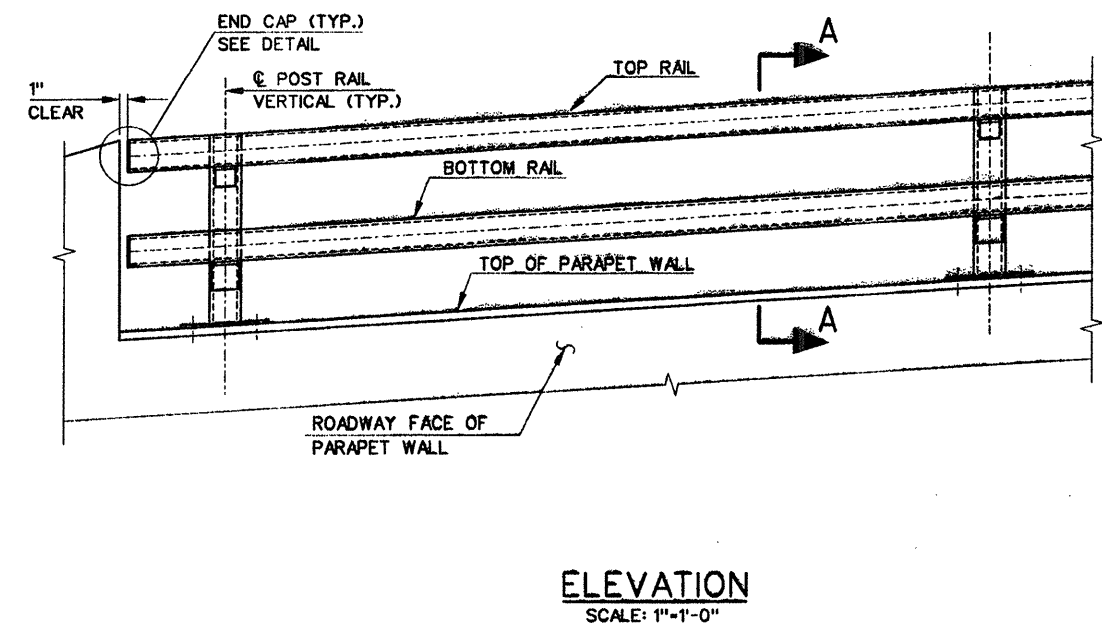
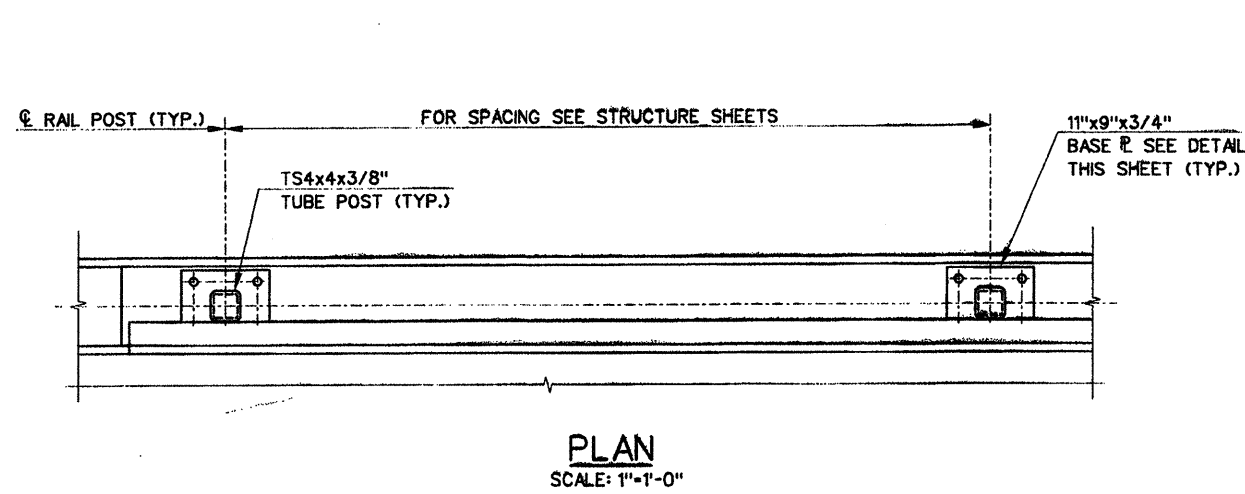
TOWN:  
**NEW HAVEN**

DRAWING TITLE:  
**METAL BRIDGE RAIL PROTECTIVE  
 FENCE (TYPE C)- SHEET 1 OF 2**

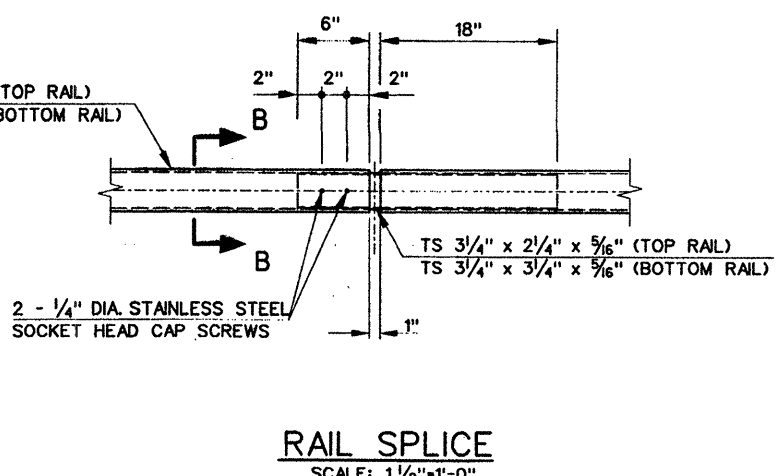
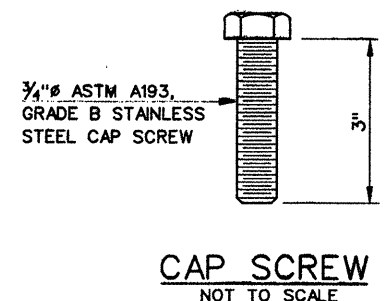
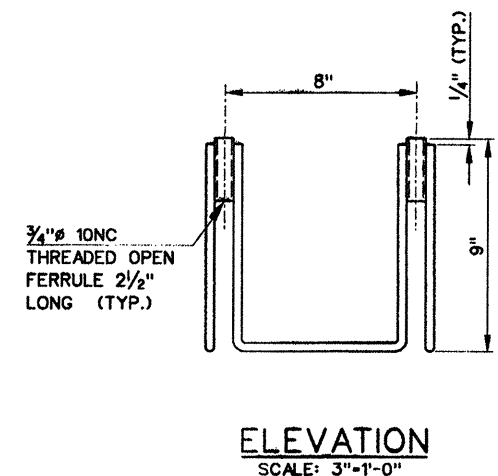
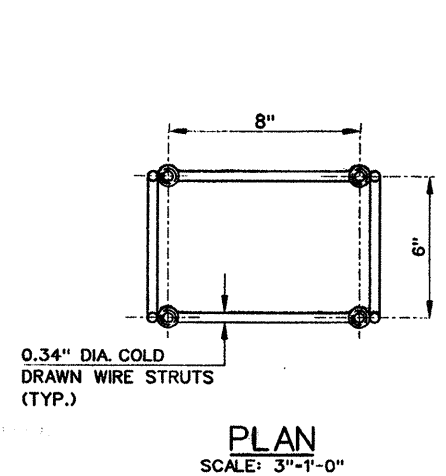
PROJECT NO.:  
 92-526

DRAWING NO.:  
 STR-107-1

SHEET NO.:  
 241-1



- NOTES**
1. THE POSTS, BASES, POST CONNECTION DEVICES, SPLICE BARS AND RAILS SHALL BE EXTRUDED ALUMINUM CONFORMING TO ASTM B221, ALUMINUM ALLOY 6061-T6 AND SHALL BE ANODIZED CLEAR.
  2. ALL CAP SCREWS SHALL BE STAINLESS STEEL AND CONFORM TO THE REQUIREMENTS OF ASTM A193, CLASS 1 OR 2, GRADE B8M (AISI TYPE 316). STAINLESS STEEL WASHERS SHALL CONFORM TO ASTM A240, TYPES 302-305.
  3. THE RAILING SHALL BE FABRICATED AND ERECTED SO THAT THE RAILINGS ARE PARALLEL TO EACH OTHER AND TO THE BARRIER CURB AND THE RAILING POSTS ARE VERTICAL.
  4. EXPOSED SURFACES OF POSTS, RAILING, CONNECTION PLATES AND ANGLES SHALL BE EXTRUDED AND SHALL BE FREE OF BURRS, IRREGULARITIES AND SHARP EDGES.
  5. THE PRESET ANCHORAGE SYSTEM SHALL BE SET PERPENDICULAR TO THE GRADE OF THE RAIL.
  6. EXPANSION JOINT IN RAILS SHALL BE ALIGNED WITH EXPANSION JOINTS IN PARAPET WALL BUT SHALL NOT BE LESS THAN 2'-0" FROM CENTER LINE OF POST
  7. LENGTH OF RAIL SHALL BE CONTINUOUS OVER A MINIMUM OF FOUR RAIL POSTS WHERE POSSIBLE AND IN NO CASE LESS THAN TWO.



**PRESET ANCHORAGE SYSTEM**

**ADDENDUM NO. 3**

09/04/06 28 NOV 2000 R:\vgn\p03\churose\addendum no.3\c3r703s134.dgn

REV.	DATE	DESCRIPTION	SHEET NO.
1	11-21-2000	ADDENDUM NO. 3 - BOLT MATERIAL	243
2	11-7-2000	ADDENDUM NO. 2 - BRIDGE RAIL COLOR CHANGED	243

SCALE AS NOTED

DESIGNER: D. CICHOWSKI  
 DRAFTER: M. OFFENBERG  
 CHECKED BY: D. GEISSERT  
 DATE CHECKED: 4-9-00

**STATE OF CONNECTICUT**  
 DEPARTMENT OF TRANSPORTATION

ENGINEER: PARSONS BRINCKERHOFF QUADE & DOUGLAS, INC.  
 APPROVED BY: Anthony A. Wrethel  
 DATE: 11/26/00

PROJECT TITLE:  
 CHURCH STREET SOUTH EXTENSION  
 OVER NEW HAVEN INTERLOCKING  
 AND RAIL YARD

CADD FILE: AD3R703S134.DGN  
 PLOTTED DATE: 11-27-00

TOWN: NEW HAVEN  
 DRAWING TITLE: METAL BRIDGE RAIL  
 (COMBINATION) (EXTRUDED ALUMINUM)

PROJECT NO.: 92-526  
 DRAWING NO.: STR-109  
 SHEET NO.: 243